



**North Carolina Department of Cultural Resources  
State Historic Preservation Office**

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Division of Archives and History  
Jeffrey J. Crow, Director

James B. Hunt Jr., Governor  
Betty Ray McCain, Secretary

November 2, 1999

MEMORANDUM

TO: William D. Gilmore, P.E., Manager  
Project Development and Environmental Analysis Branch  
Division of Highways  
Department of Transportation

FROM: David Brook *David Brook*  
Deputy State Historic Preservation Officer

RE: **US 17 from NC 43 to Mills Street, Bridgeton, T.I.P. No. R-3403, Craven County  
ER 00-7456**

We have received the Phase II Architectural Resources Survey, prepared by Scott C. Owen, for the above project and would like to comment.

On March 19, 1999, Jennifer Martin of our office met with NCDOT staff to identify properties requiring further evaluation for eligibility determinations. At that meeting it was determined that five (5) properties within the project's area of potential effect had previously been determined eligible for listing, and that the remaining properties required no further evaluation.

Upon receipt of the Phase II Architectural Resources Survey for Craven County R-3403 and further review, we determined that the following properties do require additional evaluation:

Properties numbered 6, 20 and 28 appear to be good examples of period revival architecture and retain a high degree of integrity. These houses are unusual house types for outlying areas of the county, as such their significance should be reevaluated.

Properties numbered 41, 43, 44 and 45 appear to fall within the period of significance for the Bridgeton Historic District (1905 - 1941) and should be included within the historic district boundary.

On Figure 11 the property numbered "40" is actually the location of property number 39. While property number 40 has already been determined eligible, property number 39 appears to retain a higher degree of historic integrity. Therefore, further evaluation of property number 39 is needed.

Property number 12, Store, appears to be an intact example of a vernacular commercial building dating from the first half of the twentieth century. This is a rapidly disappearing resource whose significance should be reevaluated.

William D. Gilmore  
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November 2, 1999  
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A photograph of property number 7 was omitted from the report. Please send us a photograph of this property to include in our copy of the report.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919/733-4763.

DB:ldb

cc: B. Church

bc: Brown/Alperin

county

File

RF

# **Historic Architectural Resources Survey Report**

## **Phase II (Intensive)**

**US 17 from NC 43 (SR 1482) to Mills Street, Bridgeton  
Craven County, North Carolina**

**TIP No. R-3403  
State Project No. 8.117601  
Federal Aid Project No. STPNHF-17(24)**

### **Prepared for:**

**Project Development and Environmental Analysis Branch  
North Carolina Department of Transportation  
P. O. Box 25201  
Raleigh, North Carolina 27611**

### **Prepared by:**

**Scott C. Owen  
Architectural Historian  
7200 Almeda Rd. # 915  
Houston, Texas 77054**

**August 1999**

# Historic Architectural Resources Survey Report

## Phase II (Intensive)

US 17 from NC 43 (SR 1482) to Mills Street, Bridgeton  
Craven County, North Carolina

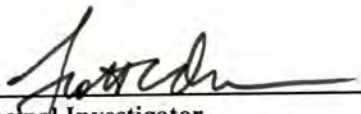
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Architectural Historian  
7200 Almeda Rd. # 915  
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\_\_\_\_\_  
Principal Investigator  
Scott C. Owen, Architectural Historian

4 Aug 99

Date

  
\_\_\_\_\_  
Supervisor, Historic Architectural Resources Section  
North Carolina Department of Transportation

8/4/99

Date



## *Management Summary*

The North Carolina Department of Transportation (NCDOT) proposes to widen US 17 from NC 43 (SR 1482) to Mills Street in Bridgeton, Craven County. NCDOT plans to widen US 17 from its current two-lane cross-section to a four-lane, curb-and-gutter facility with ten-foot berms. The length of this project is approximately eleven miles. Additional right of way will be required.

Several previous projects overlap the area of potential effects (APE) for R-3403. Ruth Little's survey for the US 17 New Bern Bypass (R-2301) in 1990 covered the northern end of the subject project in the Ernul vicinity, as did a subsequent survey in 1994, which an NCDOT staff architectural historian conducted as an addendum to Ms. Little's survey.

A second project, R-2513, also overlaps the northern end of the subject project near Ernul. In 1995, an NCDOT staff architectural historian conducted a Phase II (Abridged) survey for widening US 17 from the proposed New Bern bypass in the Askin vicinity north to the proposed Washington bypass.

In 1991, Ruth Little conducted a survey for B-2531 and B-2532, which canvassed the southern end of the subject project in Bridgeton.

In March, 1999, NCDOT staff architectural historian Carrie Albee completed a report of her Phase I (Reconnaissance) survey of historic architectural resources for R-3403. That report incorporated six properties in the APE that had been determined eligible for the National Register of Historic Places in the reports for R-2513, B-2531, and B-2532. Two of those properties, the James A. Ernul House and the Ernul Brick Road, had also been evaluated and determined eligible in Ms. Little's 1990 report for R-2301. The 1995 report for R-2513 modified the extent of the Ernul Brick Road, and included it as part of an eligible, discontinuous historic district called the New Bern-Washington Brick Road Historic District. In her Phase I survey and report, Ms. Albee also identified thirty-nine additional properties over fifty years of age. In a meeting on March 8, 1999, NCDOT and the State Historic Preservation Office (SHPO) concurred that these thirty-nine properties are not eligible for the National Register and are not worthy of further evaluation. Photographs of those properties follow in Appendix A.

In June, 1999, NCDOT hired Scott C. Owen as principal investigator to consolidate the findings of all previous reports that covered the APE for R-3403 into one Phase II (Intensive) report. This report includes the six properties within the APE that were previously determined eligible for the National Register, and that were included in the Phase I (Reconnaissance) report for R-3403. None of these properties are listed on the National Register, and one, the James A. Ernul House, is listed on the North Carolina Study List. Since Ms. Albee had already established the APE and identified and evaluated all buildings over fifty years of age within that boundary in her Phase I survey, no further survey was conducted for this Phase II report. The principal investigator consulted the Craven County survey maps and files at SHPO to ensure that no information regarding these properties was overlooked. The principal investigator also made site visits to the six eligible properties on June 14-15, 1999, to establish boundaries and prepare site maps.

Two appendices are included at the end of this report. Appendix A consists of a copy of the March 8, 1999 concurrence form, and a picture of each property that was determined not eligible for the National Register and not worthy of further evaluation. Appendix B contains a Concurrence Form for Assessment of Effects, dated August 24, 1995, regarding NCDOT's environmental commitment under R-2513 that #10 (Brick Road) of the New Bern-Washington Brick Road Historic District will not be used as a staging area for construction along US 17. That form refers to #10 (Brick Road) as Section #51, which was the property's designation in the report for R-2513.

**Properties Previously Determined Eligible for the National Register**

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New Bern-Washington Brick Road Historic District (CV 1437)	IX-10
• 5. Ernul Brick Road (CV 383)	
• 10. Brick Road (CV 1438)	
• 19. Askin Brick Road (CV 1439)	
Bridgeton Historic District (CV 369)	IX-28
• 40. House (912 C Street, Bridgeton Historic District) (CV 369)	
• 42. House (802 C Street, Bridgeton Historic District) (CV 369)	

**Properties Not Eligible for the National Register and Not Worthy of Further Evaluation**

1. House	XI-3
2. White's Strawberry Farm	XI-3
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4. House	XI-4
6. House	XI-5
7. House (photograph unavailable)	XI-5
8. House	XI-6
11. House	XI-6
12. Store	XI-7
13. House	XI-7
14. Bank's Grocery	XI-8
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16. House	XI-9
17. House	XI-9
18. House	XI-10
20. House	XI-10
21. House	XI-11
22. House	XI-11
23. House	XI-12
24. House	XI-12
25. House	XI-13

26. House  
27. House  
28. House  
29. House  
30. House  
31. House  
32. House  
33. House  
34. Store  
35. House  
36. House  
37. House  
38. House  
39. House  
41. House  
43. House  
44. House  
45. House

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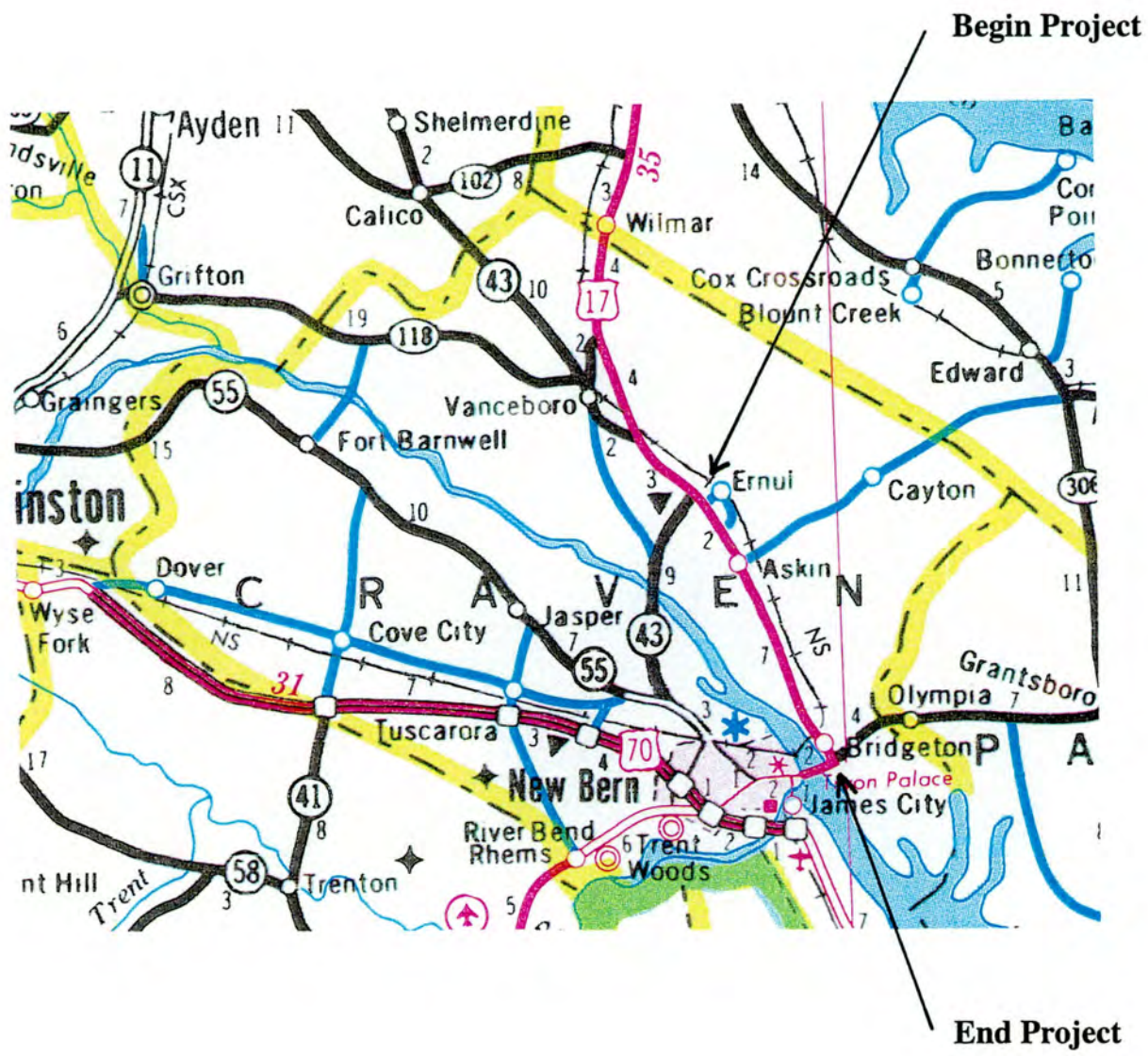
	lot. Back of ditch is western NR boundary.	
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30.	House	XI-15
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## *Introduction*

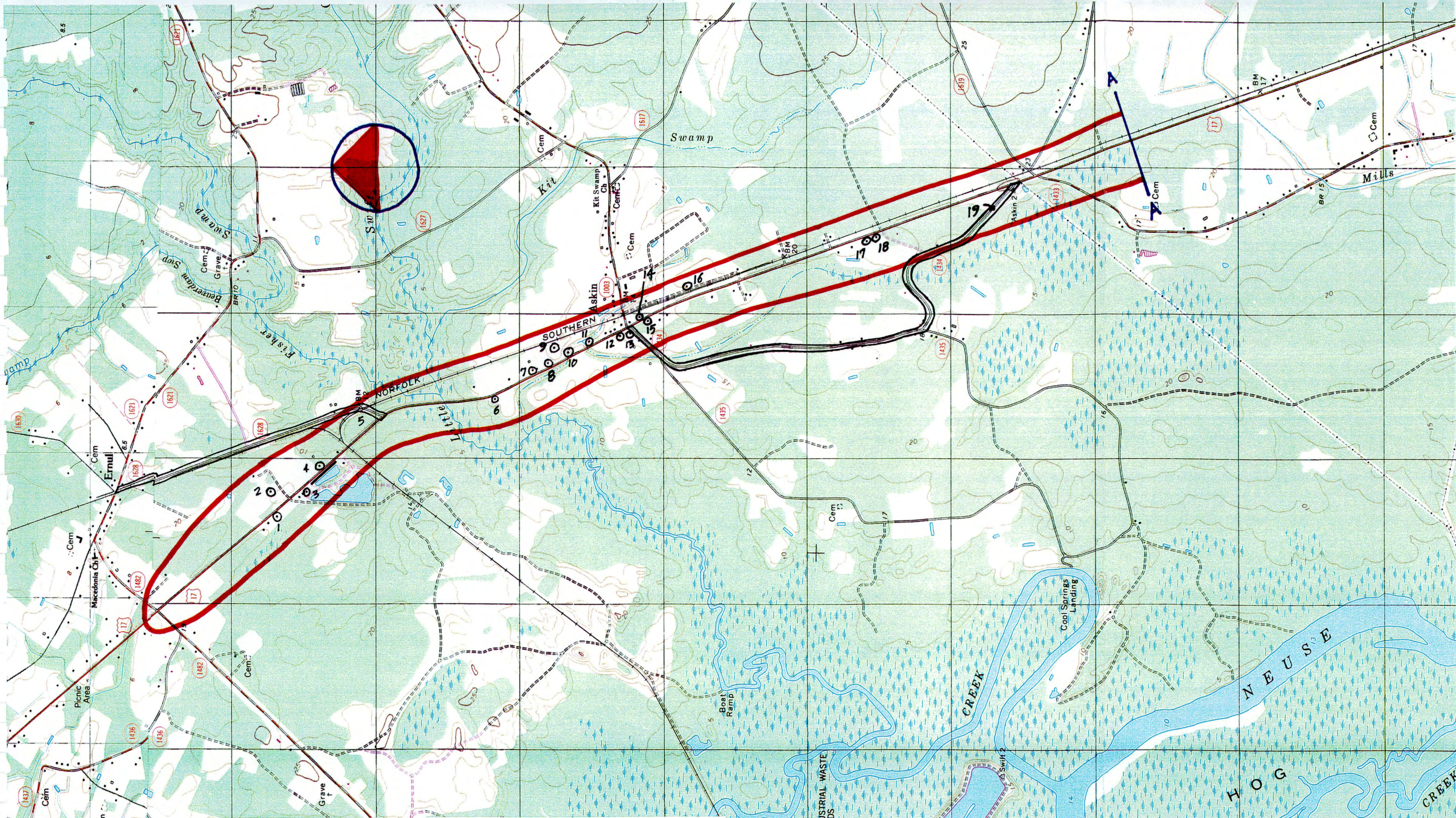
This survey was conducted and report prepared in order to identify historic architectural resources located within the APE as part of the environmental studies conducted by NCDOT and documented by an Environmental Impact Statement (EIS). This report is prepared as a technical addendum to the EIS and as part of the documentation of compliance with the National Environmental Policy Act (NEPA) and the National Historic Preservation Act of 1966, as amended. This report is on file at NCDOT and is available for review by the general public. Section 106 of the National Historic Preservation Act of 1966, as amended, 16 U.S.C. Section 470f, requires Federal agencies to take into account the effect of their undertakings on properties included in or eligible for inclusion in the National Register of Historic Places and to afford the Advisory Council on Historic Preservation a reasonable opportunity to comment on such undertakings.





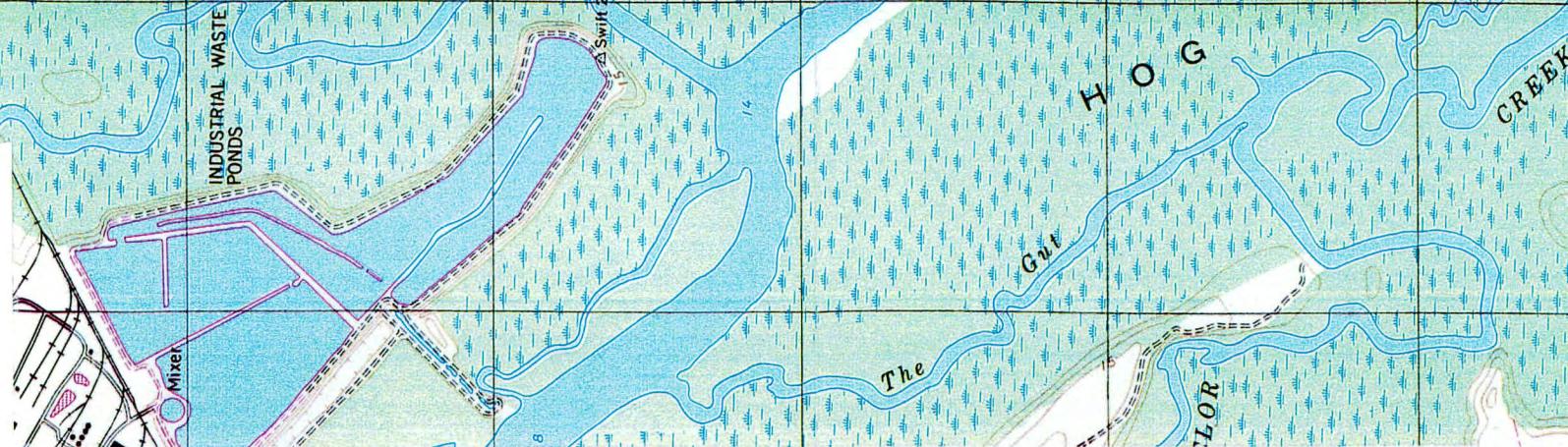
**Fig. 1 Project Area Map**  
 Approximate Scale: 1" = 6½ Miles



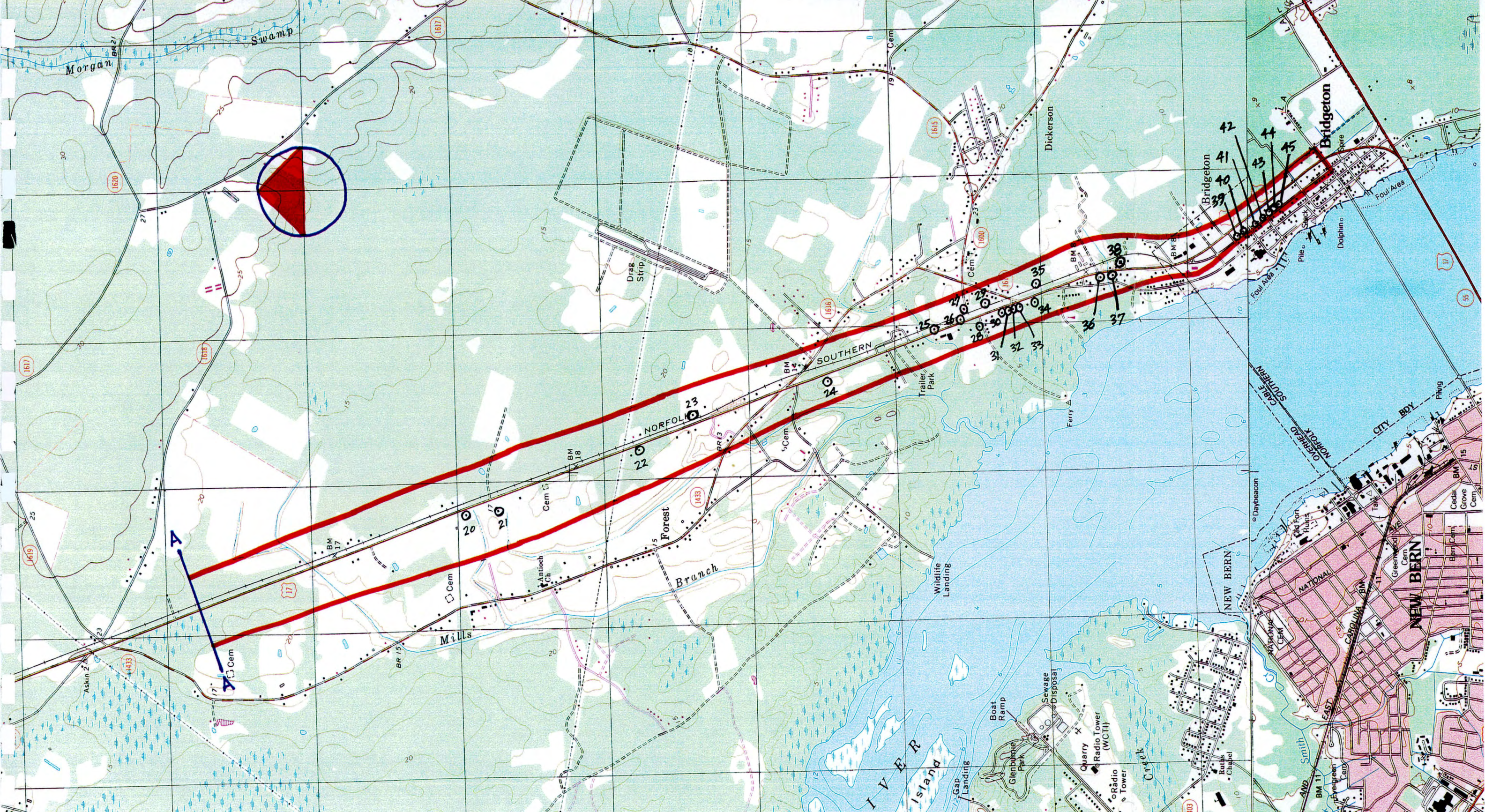


**Fig. 2 Area of Potential Effects Map**  
 Scale: 1 Inch = 2,000 Feet

**— APE Boundary**







**Fig. 2 (cont.) Area of Potential Effects Map**  
 Scale: 1 Inch = 2,000 Feet

 APE Boundary



## *Physical Environment*

Once US 17 crosses the Neuse River and enters Bridgeton, it turns north-northwest and passes through Craven County farmland on its way to Vanceboro. As US 17 leaves Bridgeton, the landscape quickly changes from small-town urban to rural, flat countryside. Modern houses among pine trees and hardwoods give way to small nineteenth- and twentieth-century farms, with fields opening up among thick woods and swamps.

US 17 parallels the Norfolk & Southern railroad tracks from Bridgeton to Askin, where the road veers northwest, away from the tracks. Much of the area around Askin on the west side of US 17 is swampy, but the road does not cross any substantial waterways until it reaches Little Swift Creek, a tributary of Swift Creek and the Neuse River north of Askin.

US 17 continues in a northwest and north-northwesterly direction until it reaches Vanceboro, outside the APE. There the road curves to the north, and continues to Washington and beyond. The only communities that US 17 passes near or through in the APE are Askin and Ernul, both of which are late-nineteenth-century settlements that later grew around the Norfolk & Southern railroad tracks.



## *Methodology*

NCDOT and its consultants conducted this Phase II (Intensive) survey, and the principal investigator compiled the report, in accordance with the provisions of FHWA Technical Advisory T 6640.8A (Guidance for Preparing and Processing Environmental and Section 4(f) Documents); the Secretary of the Interior's Standards and Guidelines for Archaeological and Historic Preservation (48 FR 44716); 36 CFR Part 800; 36 CFR Part 60; and Phase II (Intensive) Survey Procedures for Historic Architectural Resources by NCDOT dated June 15, 1994, as amended. This survey report meets the guidelines of NCDOT and the National Park Service.

NCDOT staff architectural historians and consultants conducted a Phase II (Intensive) survey with the following goals: 1) to determine the APE, defined as the geographic area or areas within which a project may cause changes in the character or use of historic properties, if any such properties exist; 2) to identify all significant historic architectural resources within the APE; and 3) to evaluate these resources according to the National Register of Historic Places criteria.

The survey methodology consisted of a field survey and historical background research of the project area. NCDOT staff architectural historians and consultants conducted the field survey by car and on foot. They photographed, investigated, and evaluated all structures over fifty years of age in the APE, and keyed all of those structures to U.S.G.S. quadrangle maps.

A search of SHPO's survey files for the project area showed that some properties within the APE have been previously surveyed. In addition, NCDOT staff architectural historians and consultants have surveyed much of the APE for several other projects: R-2301, R-2513, B-2531, and B-2532. No properties in the APE are listed on the National Register. One property, the James A. Emul House, is on the North Carolina Study List. The project area has received both reconnaissance-level and comprehensive architectural surveys in past years. Craven County was first inventoried in 1976 as part of a survey of historic and architectural resources of the Tar-Neuse River basin by the North Carolina Department of Cultural Resources. Peter Sandbeck and Angela Barnett conducted a comprehensive architectural survey of the county in 1979-80; Sandbeck published this survey in 1988 as The Historic Architecture of New Bern and Craven County, North Carolina.

## ***Historic and Architectural Background***

*\*The historic and architectural contexts for Craven County are primarily drawn from Peter Sandbeck, The Historic Architecture of New Bern and Craven County, North Carolina (1988), which is the published result of his and Angela Barnett's comprehensive survey of the county in 1980. They are reprinted here from Scott C. Owen, "US 17 from the Proposed New Bern Bypass at SR 1003 to the Proposed Washington Bypass near SR 1127, Craven and Beaufort Counties, TIP No. R-2513, State Project No. 6.179002T," unpublished material (Raleigh, NC: North Carolina Department of Transportation, June, 1995). The historic and architectural contexts for Bridgeton are reprinted from M. Ruth Little, "US 17 Neuse River Bridge and SR 1004 Trent River Bridge Replacement, EA, Craven County, State Project No. 8.1170801, T.I.P. B-2531 & B-2532," unpublished material (Raleigh, NC: North Carolina Department of Transportation, March 12, 1991, revised November 8, 1991). The reprinted material appears in quotation marks.*

### **Historical Development of Craven County**

"Settlers did not move into the southern portion of Bath County, which included all of present-day Craven County, until the late 1690's. The earliest settlements were along the Neuse River and its tributaries. Most early settlers were English. These settlers moved from Virginia, Maryland and the Albemarle region of Carolina. They were attracted by the area's cheap and readily available land. Unfortunately these early settlements were plagued with Indian attacks until the local Tuscarorans were finally subdued in 1718.

"The first effort to settle the New Bern area came in 1709 by a group of Swiss and Palatinate settlers led by Baron Christoph von Graffenried and Franz Ludwig Michel, both of Bern, Switzerland. This settlement was virtually abandoned after a series of attacks by Tuscaroran and Coree Indians between 1711 and 1718. Permanent settlement was achieved after 1720 when former governor Colonel Thomas Pollock, to whom Graffenried had mortgaged most of his land in the colony during the first year of his settlement, began selling town lots. In 1723 New Bern, now measuring about 250 acres, received township status by decree of the colonial assembly.<sup>1</sup>

"Throughout the eighteenth century Craven County's economic landscape was dominated by small farmers and landowners. Slaveholding was common, but individual slave holdings were small. During the 1750's and 1760's agriculture advanced from merely subsistence level to 'levels which produced quantities for export.' Indian corn, wheat, oats, cotton, flax, and peas were grown in large quantities, while rye and barley were harvested in smaller amounts. Local farmers raised more hogs than cattle for home consumption, and salt pork was a common export.<sup>2</sup>

"From the 1760's until the Civil War the production and export of naval stores, especially tar, pitch, and turpentine, was the mainstay of both the county and New Bern's economy. Sawmilling operations were mentioned increasingly in county records during the 1760's and 1770's. Sawn lumber, cypress shingles, and oak staves were important exports during this period.

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<sup>1</sup> Peter B. Sandbeck, The Historic Architecture of New Bern and Craven County, North Carolina, (New Bern, NC: Tryon Palace Commission, 1988) 3-5.

<sup>2</sup> Sandbeck, 454-55.

“After growing gradually through the end of the eighteenth century, Craven County, like the rest of the state, experienced a period of economic stagnation from the 1820’s until the 1850’s. As a result emigration was widespread at both the county and state level.

“The introduction of steamboats to the New Bern area in the early nineteenth century improved the ability of Craven County farmers to ship their products to market. Shallow-draft steamers were able to navigate the Neuse and Trent rivers and the lower portion of Swift Creek, and stopped at the numerous plantation landings along these waterways. Overland transportation remained difficult, but several new bridges were built during this period.”<sup>3</sup>

“New Bern continued to dominate the county throughout the antebellum period; this dominance inhibited the growth and development of smaller towns. Slaves accounted for approximately 35 percent of Craven County’s population during the period.

“Indian Corn, sweet potatoes, and Irish potatoes were the main crops. Farms and plantations remained relatively small. Most farms had 20-50 acres of cleared land and 50 to several hundred acres of uncleared land.

“Planters with the largest landholdings and distillery operations were the largest slaveholders. The production of turpentine and naval stores became a major enterprise in the county, and by 1850 the county was the largest producer of turpentine in the state.

“After New Bern was captured by the Union forces in March 1862, large numbers of slaves fled the surrounding plantations and sought refuge in town. At the war’s end blacks, who comprised 46 percent of the county’s population in 1860, resettled on the outskirts of New Bern and in surrounding counties.

“With larger landholdings gradually broken down, the number of farms increased from 690 in 1860 to a high of 2,598 in 1920. By 1920 nearly half of the county’s farms operated under the tenant system.

“During the 1880’s a number of small mills and logging operations were established in many of the smaller towns and crossroads communities in the county. The lumber industry was largely responsible for the rapid population growth between the early 1870’s and 1900. Large-scale lumber operations continued in the area until the Depression. These lumber operations required the clearcutting of vast acres of forest and the draining of the swampy terrain, and permanently changed the character of the local landscape.

“Small agricultural communities prospered during the post-war period. Truck farming began in 1880 and flourished into the early twentieth century. Corn, the main crop in antebellum Craven County, was replaced by cotton and tobacco. Cotton and tobacco production both peaked in 1920.

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<sup>3</sup> M. Ruth Little, “An Architectural Resources Survey of the US 17 New Bern Bypass, Craven County, TIP No. R-2301,” unpublished material (Raleigh, NC: North Carolina Department of Transportation, August 31, 1990) 13.

“Craven County’s substandard road network was finally improved during the early twentieth century. Between 1906 and 1909 three costly new steel drawbridges were built: one across the Neuse at Maplecyress, another across Swift Creek at Vanceboro, and a third across lower Brice’s Creek near the Trent. Several small reinforced concrete bridges were built during the period, one of which still survives at Caswell Branch (SR 1420 near Beaman’s Crossroads). Finally, the roads themselves were improved in 1916 and 1917. In 1916 ‘Lillington Cement Gravel’ had been used to pave portions of the Pembroke Road, the Neuse Road, the Trent Road and the Vanceboro Road. During 1917, the county contracted with the Georgia Engineering Company of Augusta to pave eleven miles of roads with ‘vitrified paving bricks.’ An additional twenty-two miles of roadway along the Vanceboro Road and the Neuse Road to Fort Barnwell had been contracted for. By the 1920s, this phase of road improvement was complete.”<sup>4</sup>

“Farming remained the main source of income during the Depression, with tobacco, corn, and potatoes being the leading cash crops, respectively. The lumber industry declined in importance during the 1930’s and 1940’s, as old-growth timber was depleted. By 1950, approximately 40 percent of county farms still operated under the tenant system.”

### **Architectural Development of Craven County**

#### **1690-1781**

“The 1980 comprehensive county survey identified only two pre-Revolutionary houses that remained intact: Clear Springs and Bellair. Clear Springs is a five-bay, double pile, one-and-one-half-story frame house, with four pedimented dormers, a partially raised basement, and a consistently simple interior finish. Bellair is a two-story, seven-bay, single pile plantation house on a high raised basement with a central pedimented pavilion and Flemish bond brickwork. The interior treatment is ornate. It is assumed that most of the dwellings of the period followed the basic story-and-a-half form with a hall-parlor plan.”<sup>5</sup>

#### **1781-1865**

“At the time of the 1980 survey only thirty-five antebellum buildings survived in rural Craven county. In marked contrast to the buildings constructed in New Bern during the same period, Craven County’s dwellings are simple, usually vernacular forms. Their simplicity can probably be attributed to the depressed economic conditions at the time, and the resulting wave of emigration from the county.

“From the beginning of the nineteenth century until well after the Civil War, the most popular house form in Craven County appears to have been what is described as a coastal cottage: a one- or one-and-one-half-story house with an engaged or attached shed roof front porch. Rear shed rooms were sometimes built with the original house, or added later. Typically three bays wide with a hall-parlor plan, no examples of this form have survived in the project area.

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<sup>4</sup> Little, “R-2301” 15.

<sup>5</sup> Sandbeck, 460-61.



“The county's wealthier planters built two- or two-and-one-half-story houses, one room deep, typically enlarged with rear shed rooms and an attached shed roof front porch. Most of the surviving houses of this form and period have exterior end chimneys and are three bays wide, although the finer examples have four or five bays. Interior details of these plantation houses show the influence of pattern books and skilled New Bern craftsmen.

“The only four-bay house of this form remaining in Craven County, the ca. 1832 Latham House, is located near Olympia. The dwelling has a traditional hall-parlor plan and brick end chimneys with single concave shoulders. Its simple exterior belies the finely detailed Federal style interior, which has mantels with dentil cornices, three-part friezes, gouge work, and paneled pilasters.

“By the 1840s the center hall floor plan began to replace the hall-parlor floor plan in rural Craven County. Although a few Greek Revival houses were built during the 1840s, fully-developed Greek Revival forms and details were not widely adopted until the 1850s. By the late 1850s a new Greek Revival house form had appeared: a one-story double pile cottage with a hipped roof. Only two examples of this form survived at the time of the 1980 survey. Greek Revival dwellings generally had symmetrical three-bay facades, large window openings with six-over-six or nine-over-nine sashes, and hipped roofs with wide, boxed cornices. The most fully developed extant examples have trabeated double leaf doors. The Greek Revival influence was slow to disappear in the county: two-story frame farmhouses with low-pitched gable roofs and two-story Greek Revival porticos continued to be built in the eastern part of Craven County as late as 1890.

### **1865-1929**

“Building practices in Craven County remained conservative (i. e., traditional) during the long recovery period following the Civil War. Queen Anne, Neoclassical, and Colonial Revival forms and details, popular in New Bern, were seldom built in the rural part of the county. Instead, simple single pile, three-bay, one- and two-story gable roofed dwellings were commonly built into the 1920s. Larger farmhouses of this plan were increasingly common during the period 1880-1920; by 1900, the traditional one-story rear shed rooms had been replaced by one- or two-story rear ells. Coastal cottage forms were built as late as 1911. There was a revival of the double-pile single-story form with a hip roof during the period 1890-1920, particularly for middle-class houses in small towns. After 1920 traditional forms were replaced by simple versions of the nationally popular Craftsman bungalow, built throughout the county by small-scale farmers.”

### **Historical Development of Bridgeton**

“Bridgeton is a small settlement on the Neuse River opposite New Bern that grew up around several lumber mills built in the early twentieth century. The first bridge to span the Neuse River in this location was built in 1898, and terminated at present Bridge Street. At that time only four structures stood on the site of present Bridgeton. By 1905 the Bridgeton Lumber Company had been built, and in 1907 the community incorporated as Bridgeton. In 1907-1908 the Norfolk Southern Railroad line connecting Washington to New Bern crossed the Neuse at Bridgeton. Population grew from 348 in 1910 to 548 in 1920, largely because of the construction of the

second lumber mill, the J. B. Blades Lumber Company, between 1908-1909. During this decade merchants erected stores to serve the residents and mill employees.<sup>6</sup>

“Almost all of the residential construction in Bridgeton occurred between 1905-1929 to provide housing for the merchants, farmers and mill employees drawn here by the lumber boom. The Depression closed down the two lumber companies, and the development boom was over. In the 1930s US Highway 17 was routed around Bridgeton, and the bridge was relocated to the south boundary of town. Most traffic then bypassed the business district, and the town has been in decline since that time.”<sup>7</sup>

### **Architectural Development of Bridgeton**

“Bridgeton is a primarily residential community that contains a variety of frame houses built between ca. 1910 and 1940 that relate closely to New Bern dwellings of the same period. A few of the earliest houses are substantial, stylish Queen Anne houses, but the majority are plain, modest one- and two-story side-gabled houses and Craftsman style houses. The most interesting house type in Bridgeton is the two-story gable front house, which is directly related to this traditional New Bern house type. In addition to the houses, Bridgeton has a tiny commercial district with a handful of early 20th century stores. The finest store is the Ryman Building, a two-story brick building built in 1916 at the corner of Bridge and B Streets. A few churches are scattered throughout the district. The Bridgeton Christian Church, built in 1909, is a late Gothic Revival style building with an entrance tower. The Bridgeton Methodist Episcopal Church is an ornate Gothic Revival church built in 1904 and recently brick veneered.”

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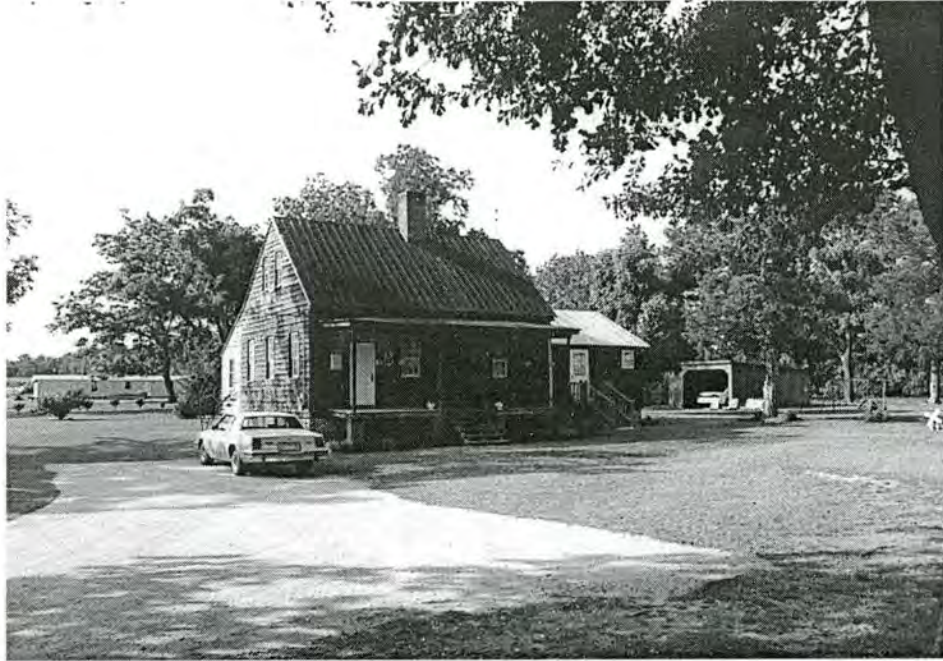
<sup>6</sup> Sandbeck, 515.

<sup>7</sup> Sandbeck, 516.

*Property Evaluations for National Register Eligibility*

**Properties Previously Determined Eligible for the National Register:**

- 9. James A. Ernul House (SL) (CV 384)**



9. James A. Ernul House. View N.



9. James A. Ernul House. View N.





9. James A. Ernul House. View E from US 17. Line of small shrubs marks northern NR boundary.



9. James A. Ernul House. View E of dirt drive marking line between Ernul house lot (PIN #2-041-047) and Ernul cemetery lot (PIN #2-041-080). Non-contributing mobile home is at R.





9. James A. Ernul House. View S of US 17 right of way in front of Ernul house lot. Back of ditch is western NR boundary.



9. James A. Ernul House. View N of US 17 right of way in front of Ernul cemetery lot.





9. James A. Ernul House. View N of Norfolk & Southern right of way. Back of ditch is eastern NR boundary.



9. James A. Ernul House. View S of Norfolk & Southern right of way. Back of ditch is eastern NR boundary. Ernul family cemetery is in background at R.

## 9. James A. Ernul House (SL) (CV 384)

*\*The description, background, and evaluation of the James A. Ernul House are reprinted from M. Ruth Little, "An Architectural Resources Survey of the US 17 New Bern Bypass, Craven County, TIP No. R-2301," unpublished material (Raleigh, NC: North Carolina Department of Transportation, August 31, 1990). The reprinted material appears in quotation marks.*

### *Description and Background*

"The James A. Ernul House is located on the northeast side of US 17 .3 mi. northwest of the junction with SR 1003 at Askin. Askin is a small turn-of-the-century crossroads settlement. The ca. 1840 house, a fine nineteenth century smokehouse, and later storage building and outhouse are sited on a 2 acre parcel that is far smaller than its original farm tract. The Norfolk & Southern Railroad tracks encroached [sic] on the site to the east and US 17 to the west. The house is a one and one-half story frame house which has an unusual form for Craven County. Its central chimney serves the main hall and parlor rooms, and to the rear is a transverse stair hall and a shed room. Two dormers, now removed, lighted the plastered attic rooms. The front porch, a shed addition at the rear, and a side kitchen ell all date from the late nineteenth century. The interior finish is remarkably well-preserved, and indicates transitional Federal-Greek Revival style. It has plaster walls, plain post-and-lintel mantels, four-panel doors, and two-part molded surrounds and chair rails. The open-string stair has a square newel, rounded handrail, and rectangular pickets typical of Craven County during this period.

"The house was probably built for James A. Ernul (1820-1896), who married Mariah Jane Dixon (1820-1882) in 1845. Both are buried in the adjacent cemetery on the property.<sup>1</sup> At the time of James Ernul's death, the farm contained 225 acres. Subsequent deeds mention such boundaries as Little Swift Creek to the west and Kit Swamp branch to the east. A 1913 deed mentions the right-of-way for Norfolk-Southern Railroad. The property was divided into small tracts, probably about this time,<sup>2</sup> and the Ernul House now sits on a small rectangular parcel in a row of twentieth century dwellings."

### *Evaluation*

"The James A. Ernul House is considered by Peter Sandbeck to be the most unusual of the small, unpretentious antebellum farmhouses surviving in Craven County. It has an unusual three-bay by three-bay central-chimney form. The plan is significant because of the unique stairhall location behind the main 'hall' room. Sandbeck thinks that this may be a rural interpretation of the side-hall gable-front form popular in New Bern during the Federal period. The house is obviously one of the finer small farmhouses of the period because of its plastered, well-finished interior. Sandbeck placed the Ernul House on the Craven County Study List in 1989. Because of its present cramped boundaries, wedged between the railroad tracks and US 17, it is important that its present boundaries be preserved."

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<sup>1</sup> The cemetery is not situated on the same parcel of land as the house. It is located on a separate tract that apparently is no longer owned by the Ernul descendants.

<sup>2</sup> The property descended through James Ernul's children and their spouses after his death in 1896. When owner James Robinson died in 1946, the farm was subdivided among the family's children.

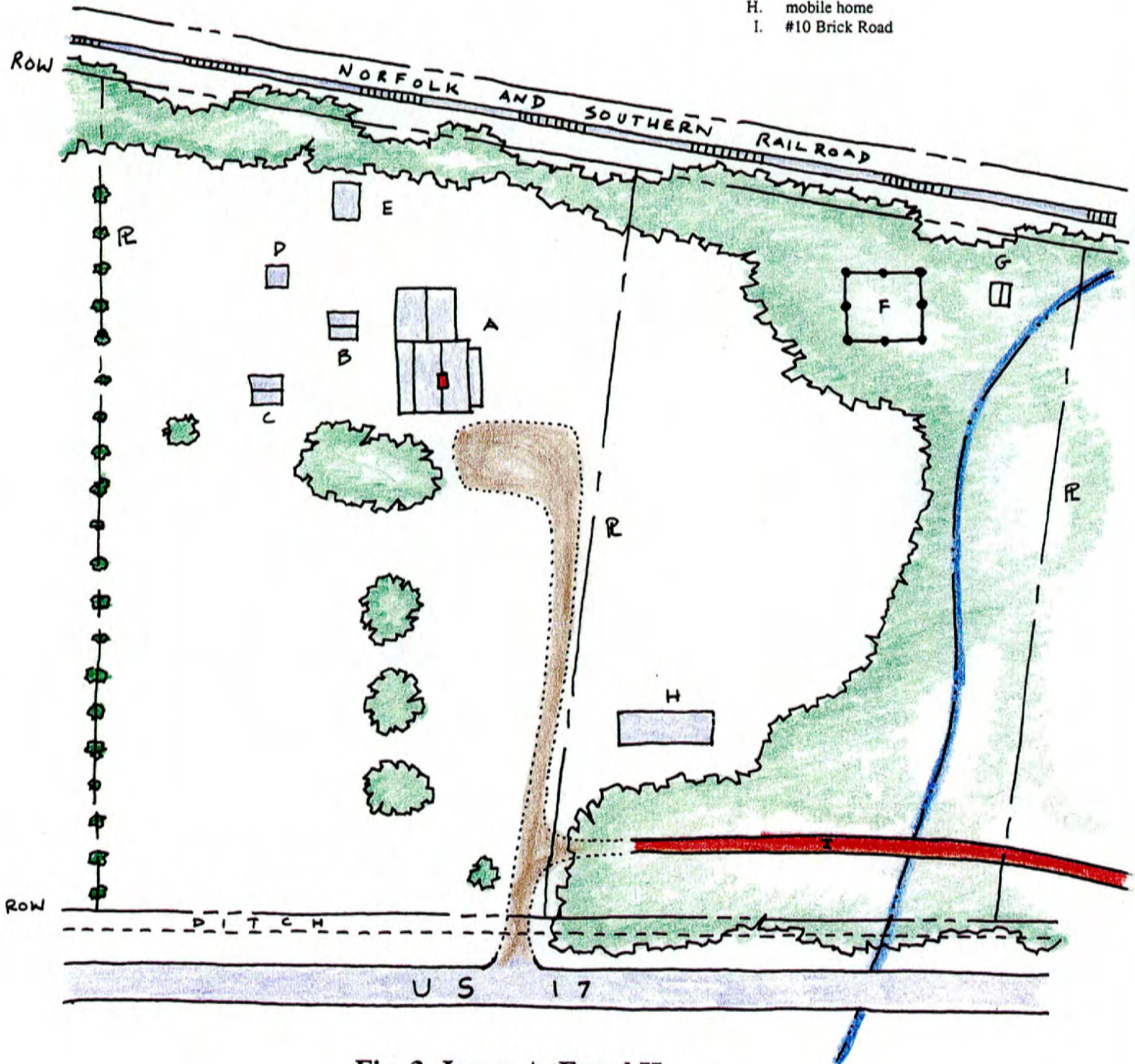
*Proposed National Register Boundary*

The proposed National Register boundary for the James A. Ernul House encompasses two lots (Craven County tax map, PIN #s2-041-047 and 2-041-080) in order to include the house, associated outbuildings, and family cemetery. The house and its associated outbuildings are located on parcel #2-041-047, and the family cemetery is located on adjoining parcel #2-041-080. The boundary is drawn to include those features historically associated with the James A. Ernul House. Property #10 Brick Road (which is part of the New Bern-Washington Brick Road Historic District) and a mobile home, both of which are on parcel #2-041-080, are included within the proposed National Register boundary as non-contributing resources. The proposed National Register boundary follows the back of the ditch on the east side of US 17, which is the edge of right of way on the highway. The boundary also follows the back of the ditch on the west side of the Norfolk & Southern railroad tracks, which is the edge of the railroad right of way.





- A. James A. Ernul House
- B. smokehouse
- C. storage building
- D. outhouse
- E. garage
- F. cemetery
- G. tobacco barn
- H. mobile home
- I. #10 Brick Road



**Fig. 3 James A. Ernul House**  
Site Plan  
Not to Scale





**Fig. 4 James A. Ernul House  
National Register Boundary Map  
Scale: 1 Inch = 280 Feet**

*Property Evaluations for National Register Eligibility*

**Properties Previously Determined Eligible for the National Register:**

**5, 10, 19. New Bern-Washington Brick Road Historic District (CV 383, 1437, 1438, 1439)**



5. Ernul Brick Road (New Bern-Washington Brick Road Historic District). View S at US 17 terminus. Original brick pavers in foreground.





5. Ernul Brick Road (New Bern-Washington Brick Road Historic District). View S.  
Typical farmstead at L.



5. Ernul Brick Road (New Bern-Washington Brick Road Historic District). View S  
showing typical rural character.



10. Brick Road (New Bern-Washington Brick Road Historic District).  
View S from N end of road section.





10. Brick Road (New Bern-Washington Brick Road Historic District). View N from S end of road section.



10. Brick Road (New Bern-Washington Brick Road Historic District). Brick pavers.





10. Brick Road (New Bern-Washington Brick Road Historic District). View S. Original brick culvert at L.



10. Brick Road (New Bern-Washington Brick Road Historic District). Original brick culvert.



10. Brick Road (New Bern-Washington Brick Road Historic District).  
View S. Note that end of road section ends inside US 17 right of way.





19. Askin Brick Road (New Bern-Washington Brick Road Historic District). View S, showing typical mix of fields and woods.



19. Askin Brick Road (New Bern-Washington Brick Road Historic District). View S, woods at L, swamp at R.





19. Askin Brick Road (New Bern-Washington Brick Road Historic District). View S, showing modern houses along road.



19. Askin Brick Road (New Bern-Washington Brick Road Historic District). View S, showing nineteenth-century farmhouse.





Ernul Brick Road, between SR 1634 and SR 1438. View S near SR 1633. Included in New Bern-Washington Brick Road Historic District for R-2513. Photo taken 1995.



Ernul Brick Road, between SR 1634 and SR 1438. Photo taken 1999 in same place as 1995. Road now widened and paved. Excluded from New Bern-Washington Brick Road Historic District for R-3403.





Ernul Brick Road, between SR 1438 and Ernul. View S near SR 1436. Included as part of Ernul Brick Road in R-2301. Excluded from New Bern-Washington Brick Road Historic District for R-2513 and R-3403 because it is now widened and paved.



Ernul Brick Road, between SR 1438 and Ernul. View S, entering Ernul. Included as part of Ernul Brick Road in R-2301. Excluded from New Bern-Washington Brick Road Historic District for R-2513 and R-3403 because it is now widened and paved.



## **5, 10, 19. New Bern-Washington Brick Road Historic District (CV 383, 1437, 1438, 1439)**

*\*The background, description, and evaluation of the New Bern-Washington Brick Road Historic District are adapted from M. Ruth Little, "An Architectural Resources Survey of the US 17 New Bern Bypass, Craven County, TIP No. R-2301," unpublished material (Raleigh, NC: North Carolina Department of Transportation, August 31, 1990), and Scott C. Owen, "US 17 from the Proposed New Bern Bypass at SR 1003 to the Proposed Washington Bypass near SR 1127, Craven and Beaufort Counties, TIP No. R-2513, State Project No. 6.179002T," unpublished material (Raleigh, NC: North Carolina Department of Transportation, June, 1995).*

### ***Description***

The New Bern-Washington Brick Road Historic District consists of three surviving sections of single-lane brick road that retain their original design and cross-section: the Ernul Brick Road (property #5), Brick Road (property #10), and the Askin Brick Road (property #19).

These three sections of brick road are part of the original brick road that ran from New Bern to Washington. Construction began in 1916, and continued for several years. The road is nine feet wide, laid with Augusta Brick pavers, and edged with concrete curbing. The New Bern-Washington Brick Road was probably paved like other brick roads in the county, which were built between 1916 and the 1920s.<sup>3</sup>

These sections of road still retain a large degree of their historic integrity. None of the sections have been widened. Brick Road (#10) still survives unpaved. Both the Ernul Brick Road (#5) and the Askin Brick Road (#19) have been paved in asphalt, which has worn away in places to reveal some of the original brick pavers. Small farms and modern houses line these roads, and dense woods and swamps reach out to the edge of the roads in many places. Few historic houses and buildings appear to survive along these sections of road. However, all three sections still retain their rural character, even where new houses have been built along the roads.

### ***Background***

The Ernul Brick Road was first determined eligible in Ruth Little's 1990 report for R-2301. In that report, Ms. Little identified the Ernul Brick Road (SR 1628) as running from SR 1438 southeast to Ernul, where it turned south-southeast and continued until it ended at US 17.

In 1995, the principal investigator, then an NCDOT staff architectural historian, conducted a survey for R-2513 that overlapped with the APEs for R-2301 and the subject project R-3403 in the Ernul vicinity. By that time, half of the Ms. Little's Ernul Brick Road, from SR 1438 southeast to Ernul, had been widened and paved. The surveyor discovered that the Ernul Brick Road returned to its original width and design northwest of SR 1438, and continued in that direction all the way to SR 1634. In addition, he identified three other sections of brick road in the R-2513 APE that survived in their original design and width. Therefore, he evaluated the four sections of brick road that survived in their original design and width as a discontinuous

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<sup>3</sup> M. Ruth Little, "An Architectural Resources Survey of the US 17 New Bern Bypass, Craven County, TIP No. R-2301," unpublished material (Raleigh, NC: North Carolina Department of Transportation, August 31, 1990) 35.

historic district, and believed that the New Bern-Washington Brick Road Historic District was eligible for the National Register. That district consisted of the following:

- Ernul Brick Road [identified as section #48], SR 1628 from SR 1634 southeast to SR 1438, approximately 1.6 miles long;
- Ernul Brick Road [identified as section #48a], SR 1628 from Ernul south-southeast to US 17, approximately 1.2 miles long;
- Brick Road [identified as section #51], east side of US 17 approximately 750 feet north of SR 1003, approximately 0.1 mile long; and
- Askin Brick Road [identified as section #54], SR 1434 from US 17 near SR 1003 south to US 17 near SR 1619, approximately 2.5 miles long.

A June, 1999, site visit for subject project R-3403 revealed that the section of Ernul Brick Road identified as section #48 in the report for R-2513 has also been widened and paved. In each place where the Ernul Brick Road has been widened and paved, that particular section has lost its integrity and no longer exhibits the characteristics of an early-twentieth-century, single-lane brick road. In those places, the original road bed has been lost beneath a two-lane strip of asphalt, and the dense forest that closely lined the road in many places has been cut back. Where neighbors once had to pull over to let each other slowly pass along the narrow one-lane road, heavy trucks now fly by at more than fifty-five miles an hour. The widening of the road has led to increased traffic, which in turn has had a dramatic, adverse impact on the road's rural character and feeling. Therefore, the discontinuous historic district has been reduced to include only those three surviving sections of brick road that retain their original design and historic character. The district now consists of the following:

- Ernul Brick Road [identified in this report as #5], SR 1628 from Ernul south-southeast to US 17, approximately 1.2 miles long;
- Brick Road [identified in this report as #10], east side of US 17 approximately 750 feet north of SR 1003, approximately 0.1 mile long; and
- Askin Brick Road [identified in this report as #19], SR 1434 from US 17 near SR 1003 south to US 17 near SR 1619, approximately 2.5 miles long.

### *Evaluation*

These three sections of brick road have been evaluated as a discontinuous historic district within the historic context of Craven County. A district possesses a significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development. A discontinuous district is further defined as being composed of two or more definable significant areas separated by nonsignificant areas, and is considered appropriate when: 1) the elements are spatially discrete; 2) space between the elements is not related to the significance of the district; and 3) visual continuity is not a factor in the significance.<sup>4</sup>

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<sup>4</sup> U. S. Department of the Interior, How to Apply the National Register Criteria for Evaluation, National Register Bulletin 15, 2nd ed., 1991 (Washington, D. C.: GPO, 1994) 5-6.

The three separate sections of the New Bern-Washington Brick Road have been determined eligible for the National Register as a discontinuous historic district under Criterion A: Transportation. A property can be eligible if it is associated with "a pattern of events or a historic trend that made a significant contribution to the development of a community, a State, or the nation."<sup>5</sup> The New Bern-Washington Brick Road satisfies this requirement, as it was part of the first paved network of roads in Craven County. This network was instrumental in improving transportation in the county, and was part of a larger effort to improve transportation in North Carolina.

*Proposed National Register Boundary*

The proposed National Register boundary for the New Bern-Washington Brick Road Historic District encompasses each of the three discontinuous road sections that make up the district. The boundaries for the Ernul Brick Road (#5) and the Askin Brick Road (#19) follow the edges of right of way, which are shown to be the backside of the drainage ditches, and the treelines where there are no drainage ditches. Since no right of way along Brick Road (#10) exists today, the boundary for that section follows the original shoulder, which is still visible in some places, approximately five feet on both sides of the brick paving. The boundary for Brick Road (#10) also includes the original brick culvert where the road crosses a creek. Since the southern end of this section of road extends approximately ten feet into current right of way on the east side of US 17, the proposed National Register boundary necessarily includes some of that right of way.

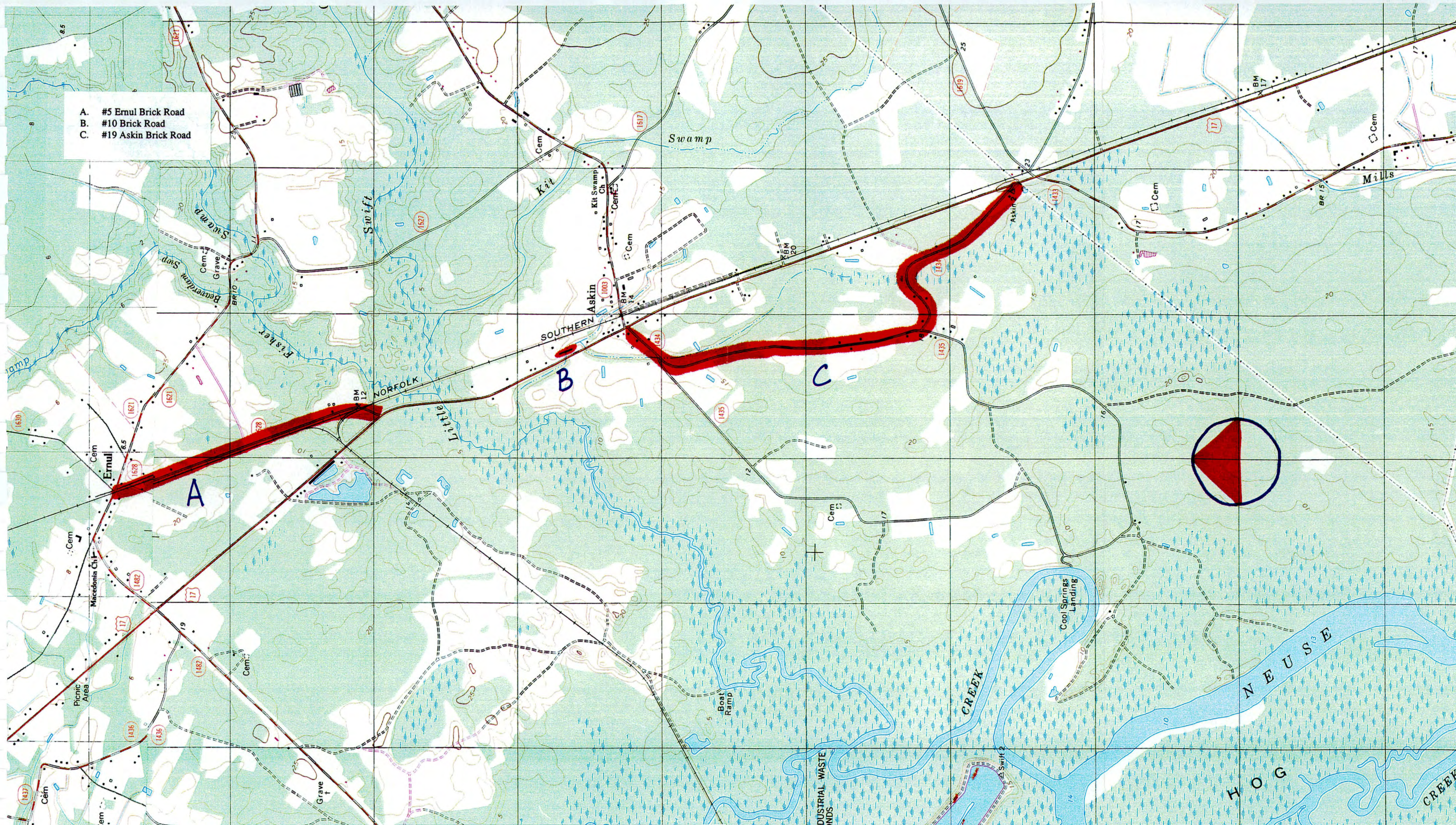
NCDOT has made an environmental commitment regarding part of the New Bern-Washington Brick Road Historic District in a previous project. On August 24, 1995, NCDOT and SHPO signed a Concurrence Form for Assessment of Effects for R-2513, wherein NCDOT pledged not to use Brick Road (#10) as a staging area for construction along US 17.<sup>6</sup> This form is included in Appendix B.

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<sup>5</sup> How to Apply the National Register Criteria for Evaluation, 12.

<sup>6</sup> This concurrence form refers to Brick Road (#10) as Section #51, which was the property's designation in the report for R-2513.

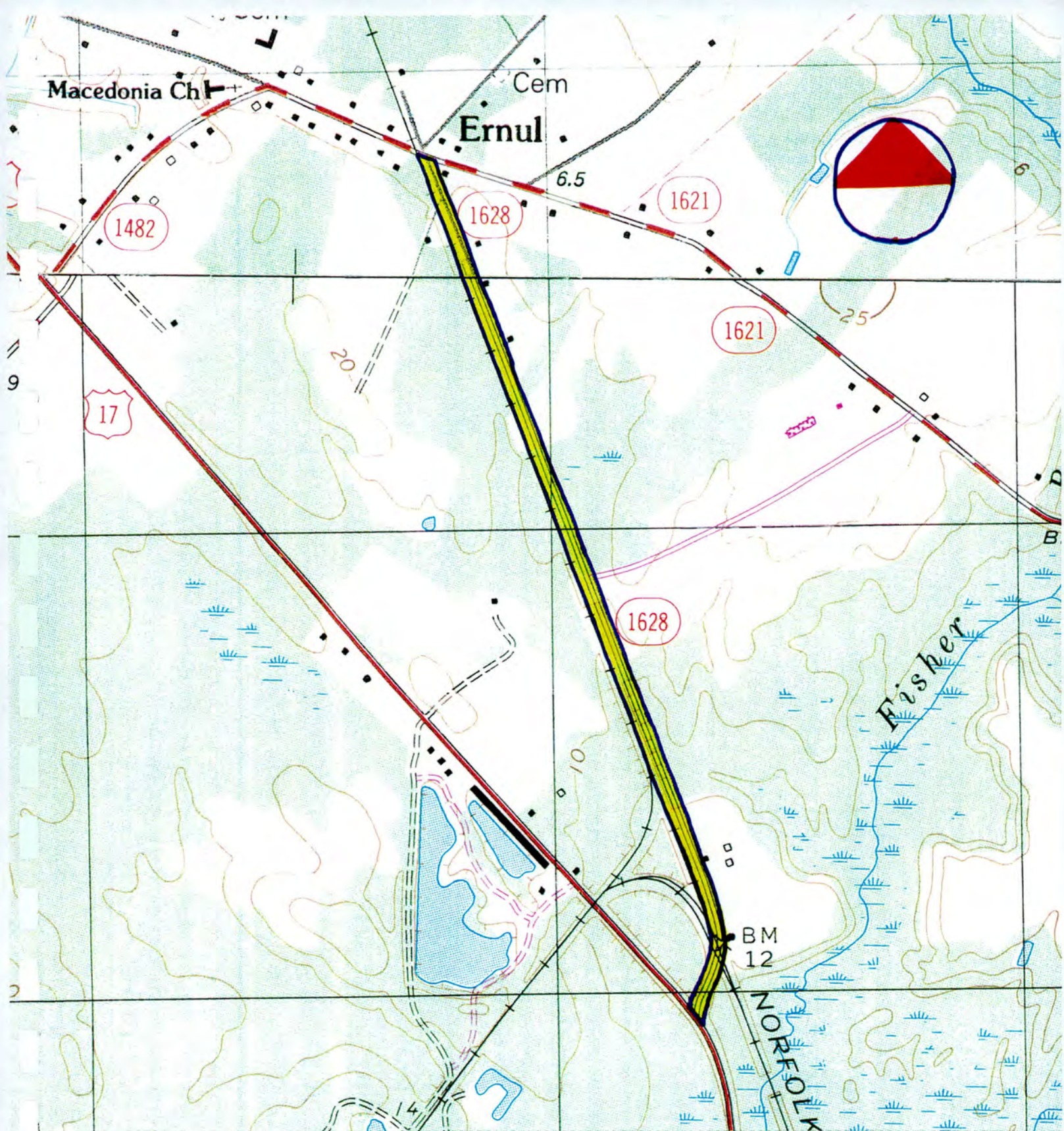




- A. #5 Ernul Brick Road
- B. #10 Brick Road
- C. #19 Askin Brick Road

**Fig. 5 New Bern-Washington Brick Road Historic District  
Site Plan  
Scale: 1 Inch = 2,000 Feet**

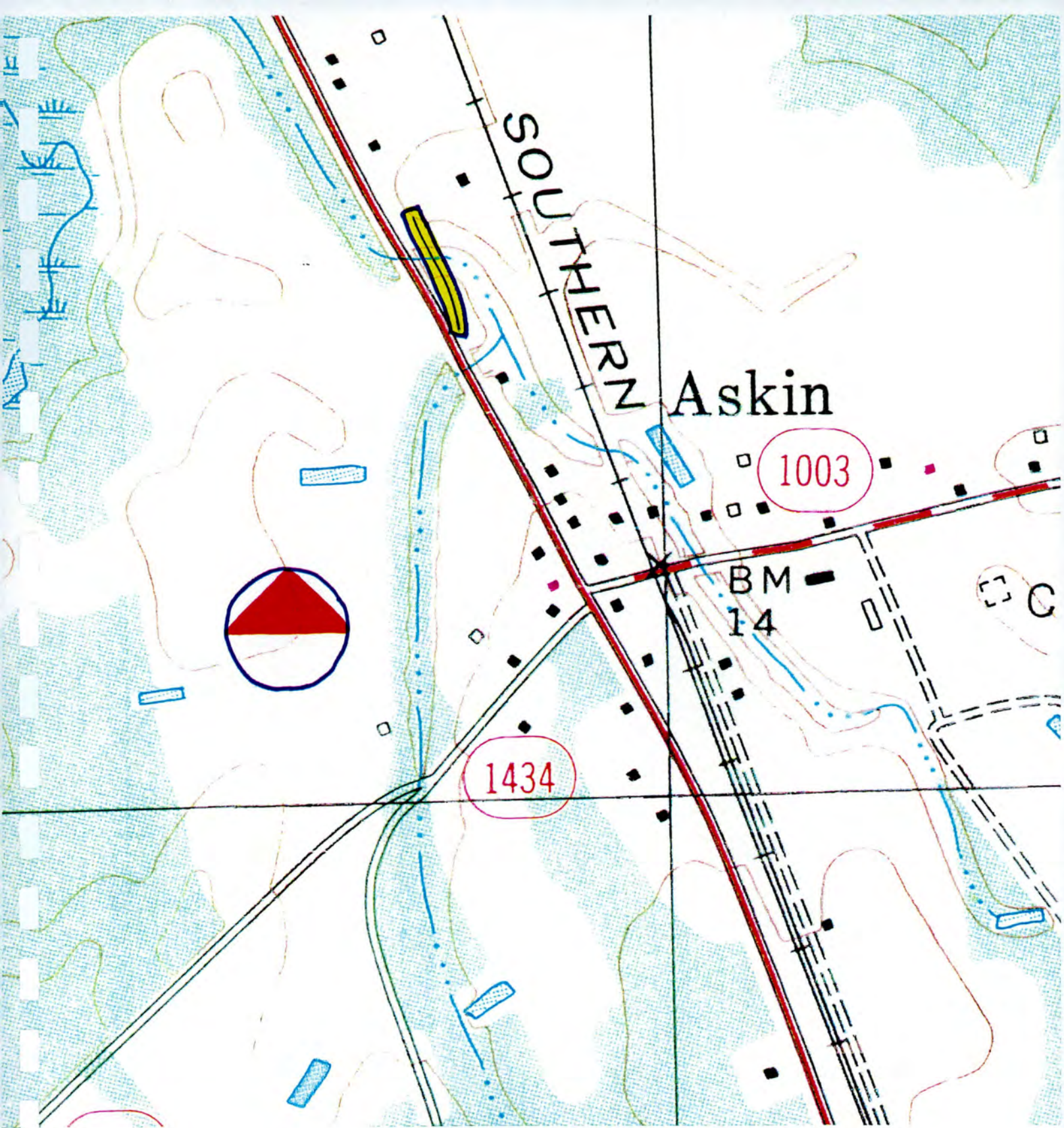




**Fig. 6 #5 Ernul Brick Road**  
**(New Bern-Washington Brick Road Historic District)**  
**National Register Boundary Map**  
**Approximate Scale: 1 Inch = 850 Feet**

CV 303

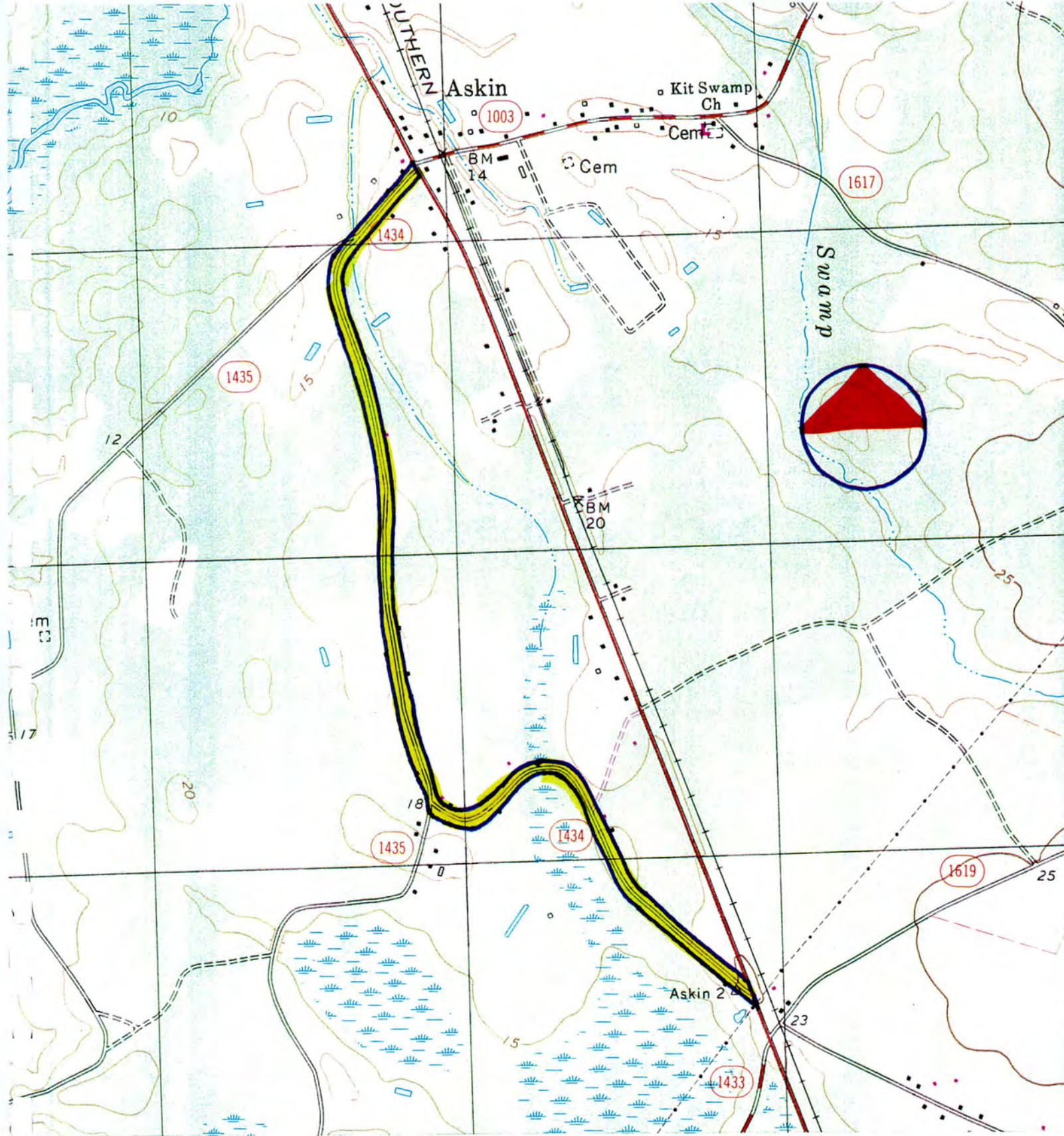




**Fig. 7 #10 Brick Road**  
**(New Bern-Washington Brick Road Historic District)**  
**National Register Boundary Map**  
**Approximate Scale: 1 Inch = 500 Feet**

CV 1438





**Fig. 8 #19 Askin Brick Road**  
**(New Bern-Washington Brick Road Historic District)**  
**National Register Boundary Map**  
**Approximate Scale: 1 Inch = 3,000 Feet**

CV 1439

*Property Evaluations for National Register Eligibility*

**Properties Previously Determined Eligible for the National Register:**

**#40 House, 912 C Street, Bridgeton Historic District (CV 369)**

**#42 House, 802 C Street, Bridgeton Historic District (CV 369)**





40. House, 912 C Street, Bridgeton Historic District. View E.



40. House, 912 C Street, Bridgeton Historic District. View W.





40. House, 912 C Street, Bridgeton Historic District. View E from rear of house to US 17.



40. House, 912 C Street, Bridgeton Historic District. View W from US 17 to rear of house.





42. House, 802 C Street, Bridgeton Historic District. View N.



42. House, 802 C Street, Bridgeton Historic District. View E.





42. House, 802 C Street, Bridgeton Historic District. View N.



42. House, 802 C Street, Bridgeton Historic District. View S along US 17. House is marked with arrow at R



**#40 House, 912 C Street, Bridgeton Historic District (CV 369)**

**#42 House, 802 C Street, Bridgeton Historic District (CV 369)**

*\*The description, background, and evaluation of the Bridgeton Historic District are reprinted from M. Ruth Little, "US 17 Neuse River Bridge and SR 1004 Trent River Bridge Replacement, EA, Craven County, State Project No. 8.1170801, T.I.P. B-2531 & B-2532," unpublished material (Raleigh, NC: North Carolina Department of Transportation, March 12, 1991, revised November 8, 1991). The reprinted material appears in quotation marks.*

*Description and Background of Bridgeton Historic District*

"Bounded by the Neuse River on the west, Academy Street on the north, C Street on the east, and with an irregular boundary on the south, the Bridgeton Historic District contains approximately 109 buildings located on grid-patterned streets. The district extends three blocks east from the Neuse River and extends from South Street on the south to Academy Street seven blocks to the north. The district contains houses, commercial buildings, churches and institutional buildings built primarily during the town's boom period of 1905-1929. During this period there were two lumber mills, the Blades Lumber Company at the north end of town on the river bank just north of Pine St., and the Bridgeton Lumber Company, located on the river bank just north of Mill Street. Both have been demolished. Although both mills had some mill houses for low-level employees, none of these houses appear to have survived. The surviving housing is substantial one and two-story frame houses built for the merchants, farmers, mill employees and others who were attracted to Bridgeton by the lumber boom. Bridgeton, as it appears today, is therefore an unusual type of mill town – middle class rather than blue collar.

"Of the 109 buildings which constitute the densest concentration of over-fifty year old buildings in Bridgeton, sixty-nine are 'contributing,' meaning that they are over fifty-years-old and retain their architectural integrity. Approximately 15 of the earliest houses, which may date from the decade of the 1910s, belong to the two-story gable front type which was a traditional house type in nearby New Bern from the late eighteenth century into the early twentieth century. This house type is therefore an important vernacular house form for Bridgeton. There are a small number of decorative and sizeable Queen Anne style houses of the type associated with successful early twentieth century merchants. A number of the pre-1941 houses are bungalow and Craftsman style houses built in the 1920s and 1930s.

"During the town's heyday, the major north-south highway, known as the New Bern to Vanceboro Road, was B Street. Between 1910 and 1920 a small business district developed where B Street crossed Bridge Street, the road leading across the Neuse River Bridge to New Bern. The Ryman and Tingle Buildings, the two most substantial commercial buildings in Bridgeton, still stand at this intersection. Just west of this intersection, on Bridge Street, is the former Bank of Bridgeton building. A few commercial buildings and small stores are scattered throughout the rest of the town.

"The Depression closed down Bridgeton's two major lumber companies, and town building activity ceased. In the 1930s the north-south highway, named US 17, was routed around the town on original 'D' street, and the bridge was moved down to the south end of town. The town has been bypassed by through traffic since the 1930s, and most commercial activity has shifted

down to NC 55 at the south boundary and to US 17 on the east boundary. Bridgeton has been a sleepy bedroom community for New Bern for much of the past fifty years, and because of this lack of commercial progress the town retains a significant degree of early twentieth century architectural character.”

#### *Description and Background of #40 House and #42 House*

Property #40 (912 C Street) is a two-story, gable-front, frame house with rear additions, thought to be built in the 1910s. It exhibits a form that is similar to other houses of the same period in Bridgeton and New Bern. The original front entrance has been moved to the side. The original weatherboarding, which survives in the enclosed porch of the rear ell, has been covered in asbestos shingles. Ms. Little thought that this house might have originally been a side hall house, and determined that it is a contributing resource for the Bridgeton Historic District.<sup>7</sup>

Property #42 (802 C Street) is a 1910s two-story, single-pile frame house, with a two-story rear ell. A double-height portico with gable returns has replaced the original porch. An original door in the second floor opens onto a small balcony under the replacement porch. Aluminum siding now covers the original weatherboards. Ms. Little determined this house to be a contributing resource for the Bridgeton Historic District.<sup>8</sup>

#### *Evaluation*

“The proposed Bridgeton Historic District is a well-preserved early twentieth century community that has local significance both for its architecture and its history. The Bridgeton Historic District is eligible for the National Register under Criterion A, for community development for the 1905-1941 period. Although the majority of residential and commercial construction in the district occurred between 1905-1929, a small group of buildings dating from the 1930s are located in the district. The only two surviving filling stations in the district, the large brick station at the southeast corner of Bridge and C streets . . . and the frame combination station/residence at 513 C Street . . . are believed to date from the 1930s. The filling station, which serviced the emerging automobile industry, is an integral component of an early twentieth century community and an endangered building type. The rest of the 1930s buildings are modest front gabled Craftsman houses which illustrate the economic decline of the district during this era.

“The proposed Bridgeton Historic District is eligible under Criterion C for its significant concentration of early twentieth century architecture. Sixty-three percent of the 109 buildings in the district were built during the period of significance, 1905-1941, and have retained sufficient integrity of location, design, materials, setting, workmanship, feeling, and association to be contributing resources. A number of them have such typical alterations as artificial siding and replacement porches but these changes have destroyed the architectural character of the buildings

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<sup>7</sup> M. Ruth Little, “US 17 Neuse River Bridge and SR 1004 Trent River Bridge Replacement, EA, Craven County, State Project No. 8.1170801, T.I.P. B-2531 & B-2532,” unpublished material (Raleigh, NC: North Carolina Department of Transportation, March 12, 1991, revised November 8, 1991) 24.

<sup>8</sup> Little, “US 17 Neuse River Bridge” 24.



in only a few instances. Scattered throughout the proposed Bridgeton district are a group of substantial, well-preserved two-story front gabled houses which reflect the influence of New Bern. This vernacular house type is dominant in New Bern's housing stock from the late eighteenth to the early twentieth centuries. Another important group of buildings are the two-story Queen Anne style houses built by Bridgeton's merchants. A third important group of landmarks are the well-preserved one and two-story brick commercial buildings built at the intersection of B and Bridge streets between 1910 and 1920.

"The remaining contributing buildings in the proposed district are modest dwellings, but the ensemble is most significant because it represents an entire town rather than the more usual historic district which is only a portion of a town. Most historic districts contain either a portion of a residential area, or a portion of a commercial area. Stores and houses rarely survive in their original relationship to each other as they have in the proposed Bridgeton district, because of the tendency of a commercial district to spread outward, thereby demolishing the early residential fabric which surrounded it."

*Proposed National Register Boundary*

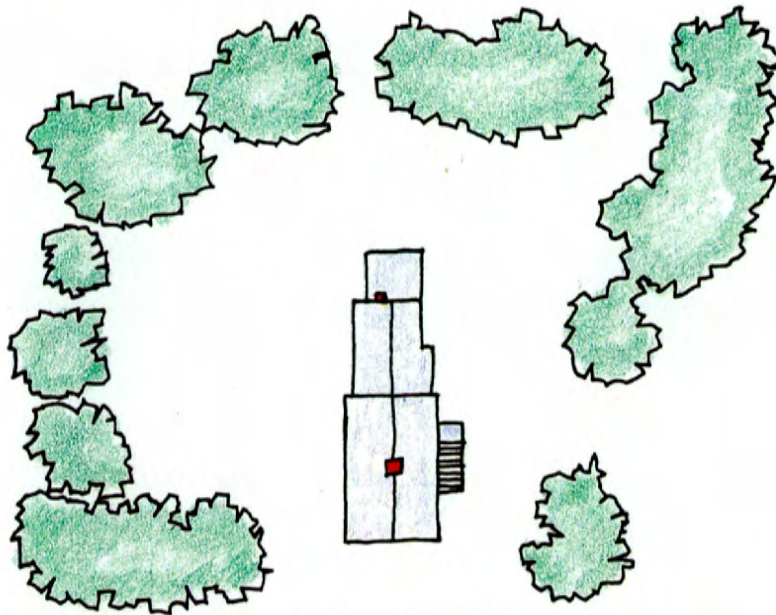
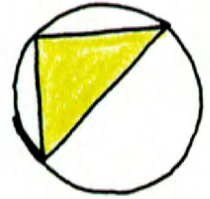
The proposed National Register boundary for the Bridgeton Historic District has an irregular outline; it is generally bounded by Academy Street on the north, C Street on the east, the Neuse River on the west, and an irregular line between New Street and South Street on the south. This boundary was drawn to encompass all the contributing resources in the district. It includes 109 structures, sixty-nine of which were determined to be contributing.

This boundary was established, though not justified, by Ms. Little in her report for B-2531 and B-2532. The principal investigator for subject project R-3403 concurs with Ms. Little's proposed boundary for the Bridgeton Historic District, as well as with her determination that #40 (912 C Street) and #42 (802 C Street) are contributing resources for the district. The principal investigator also concurs with Ms. Little's exclusion of #s 41, 43, 44, and 45 from the district.

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U S 17

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C STREET

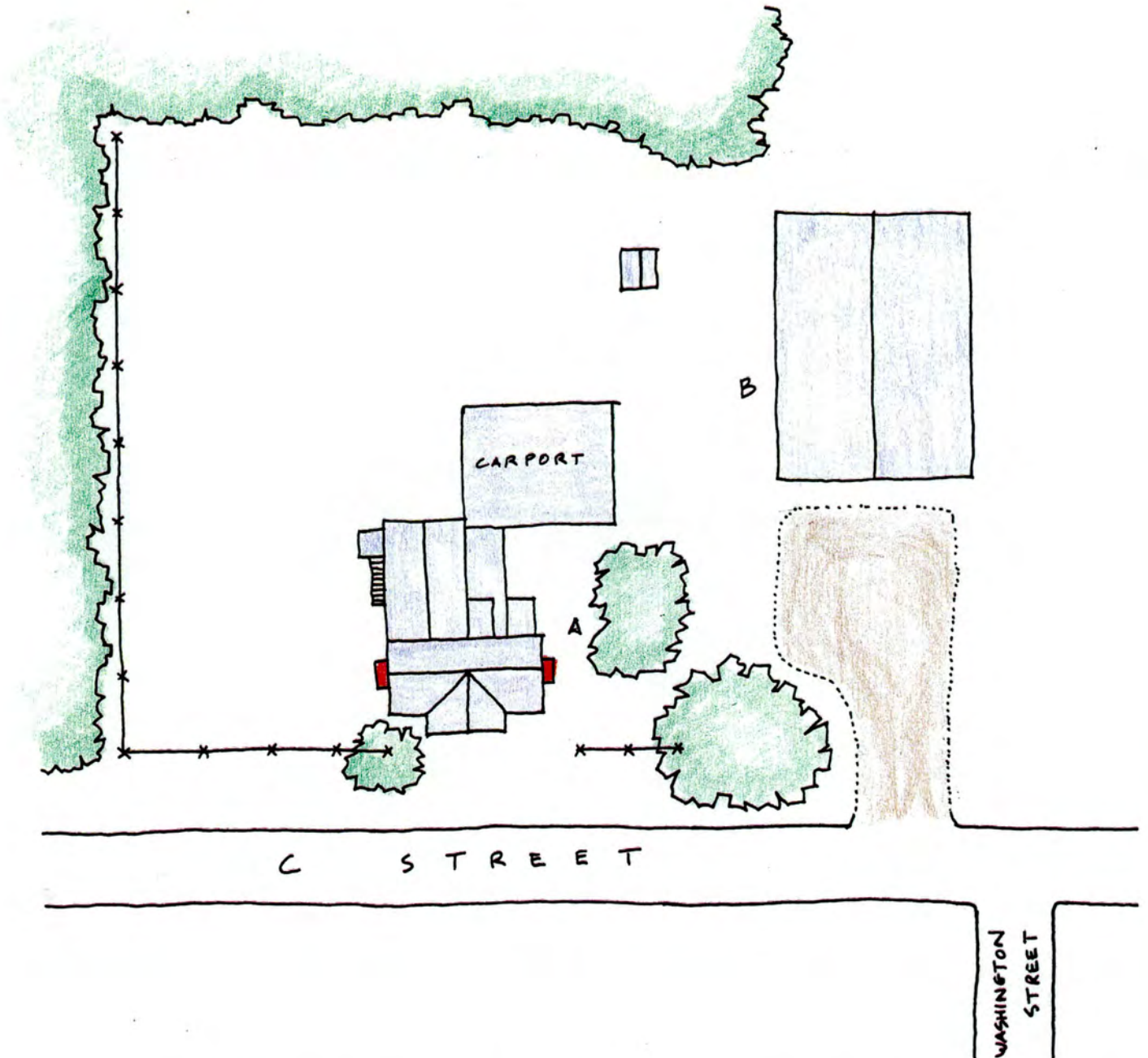
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**Fig. 9 #40 House, 912 C Street, Bridgeton Historic District**  
Site Plan  
Not to Scale





- A. #42 House
- B. Bridgeton Rescue



**Fig. 10 #42 House, 802 C Street, Bridgeton Historic District**  
Site Plan  
Not to Scale







## *Bibliography*

- Little, M. Ruth. "An Architectural Resources Survey of the US 17 New Bern Bypass, Craven County, TIP No. R-2301." Unpublished material. Raleigh, NC: North Carolina Department of Transportation, August 31, 1990.
- . "US 17 Neuse River Bridge and SR 1004 Trent River Bridge Replacement, EA, Craven County, State Project No. 8.1170801, T.I.P. B-2531 & B-2532." Unpublished material. Raleigh, NC: North Carolina Department of Transportation, March 12, 1991, revised November 8, 1991.
- Owen, Scott C. "US 17 from the Proposed New Bern Bypass at SR 1003 to the Proposed Washington Bypass near SR 1127, Craven and Beaufort Counties, TIP No. R-2513, State Project No. 6.179002T." Unpublished material. Raleigh, NC: North Carolina Department of Transportation, June, 1995.
- Sandbeck, Peter C. The Historic Architecture of New Bern and Craven County, North Carolina. New Bern, NC: Tryon Palace Commission, 1988.

*Appendix A*

**Concurrence Letter and Photographs  
of Properties Not Eligible for the National Register  
and Not Worthy of Further Evaluation  
(March 18, 1999)**



Federal Aid # 8.117601 TIP # R-3403 County Craven

CONCURRENCE FORM FOR PROPERTIES NOT ELIGIBLE FOR  
THE NATIONAL REGISTER OF HISTORIC PLACES

Brief Project Description Widen US 17 from Enul to Bridgeton

On 3-18-99, representatives of the

- North Carolina Department of Transportation (NCDOT)
- Federal Highway Administration (FHWA)
- North Carolina State Historic Preservation Office (SHPO)
- Other \_\_\_\_\_

reviewed the subject project at

- A scoping meeting
- Historic architectural resources photograph review session/consultation
- Other \_\_\_\_\_

All parties present agreed

- there are no properties over fifty years old within the project's area of potential effects.
- there are no properties less than fifty years old which are considered to meet Criterion Consideration G within the project's area of potential effects.
- there are properties over fifty years old (list attached) within the project's area of potential effects, but based on the historical information available and the photographs of each property, properties identified as 1-4, 6-8, 11-18, 20-39, 41, 43-45 are considered not eligible for National Register and no further evaluation of them is necessary.
- there are no National Register-listed properties within the project's area of potential effects.

Signed:

Cassie Allen \_\_\_\_\_ 3-18-99  
Representative, NCDOT Date

Ray C. Shelton \_\_\_\_\_ 3-19-99  
FHWA, for the Division Administrator, or other Federal Agency Date

J. F. Mat. \_\_\_\_\_ 3/18/99  
Representative, SHPO Date

[Signature] \_\_\_\_\_ 4/1/99  
State Historic Preservation Officer Date

If a survey report is prepared, a final copy of this form and the attached list will be included.



1. House



2. White's Strawberry Farm





3. House



4. House



6. House

7. House (photograph unavailable)





8. House



11. House





12. Store



13. House





14. Bank's Grocery



15. James Virgil Thomas House (CV 324)





16. House



17. House





18. House



20. House





21. House



22. House





23. House



24. House





25. House



26. House





27. House



28. House





29. House



30. House





31. House



32. House





33. House



34. Store





35. House



36. House





37. House



38. House





39. House



41. House



43. House



44. House





45. House

*Appendix B*

**Concurrence Form for Assessment of Effects, R-2513  
(August 25, 1995)**



Federal Aid # N/A TIP # R-2513 County Craven-Bearfort

**CONCURRENCE FORM  
FOR  
ASSESSMENT OF EFFECTS**

Brief Project Description

US 17 from Proposed New Bern Bypass @ SR 1003  
to Proposed Washington Bypass near SR 1127

On August 24, 1995, representatives of the

- North Carolina Department of Transportation (NCDOT)
- Federal Highway Administration (FHWA)
- North Carolina State Historic Preservation Office (SHPO)
- Other \_\_\_\_\_

reviewed the subject project and agreed

there are no effects on the National Register-listed property within the project's area of potential effect and listed on the reverse.

there are no effects on the National Register-eligible properties located within the project's area of potential effect and listed on the reverse.

there is an effect on the National Register-listed property/properties within the project's area of potential effect. The property-properties and the effect(s) are listed on the reverse.

there is an effect on the National Register-eligible property/properties within the project's area of potential effect. The property/properties and effect(s) are listed on the reverse.

Signed:

[Signature] 8.24.95  
Representative, NCDOT, Historic Architectural Resources Section Date

N/A  
FHWA, for the Division Administrator, or other Federal Agency Date

[Signature] 8-24-95  
Representative, SHPO Date

[Signature] 8/25/95  
State Historic Preservation Officer Deputy Date

(over)

Federal Aid # N/A TIP # R-2513 County Craven-Beaufort

Properties within area of potential effect for which there is no effect. Indicate if property is National Register-listed (NR) or determined eligible (DE).

Norm Ipock House (DE)

James A. Ernul House (DE)

Wihmar Fire Tower + Keeper's House (DE) - upon condition of hold the edge of right of way in front of the property and widening on the other side.

New Bern - Washington Brick Road Historic District (DE) - upon condition of inclusion in the EA of an environmental commitment stating that Section #51 will not be used as a staging area for construction.

Properties within area of potential effect for which there is an effect. Indicate property status (NF or DE) and describe effect.

Reason(s) why effect is not adverse (if applicable).

Initialed: NCDOT AW FHWA \_\_\_\_\_ SHPO \_\_\_\_\_



*Appendix C*

**Professional Qualifications**

# Scott Campbell Owen

7200 Alameda Rd. # 915  
Houston, Texas 77054

Home (713) 383-7580  
Mobile (919) 632-6077

August 1999

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## PROFESSIONAL EXPERIENCE

### **Scott C. Owen, Architectural Historian**

**July 1997 to present**

Historic Preservation and Environmental Review Services  
Raleigh, North Carolina and Lewisville, Texas

#### Private Consulting Architectural Historian

Secure compliance with state and Federal historic preservation laws and regulations with respect to historic architectural properties for transportation projects; perform architectural field surveys; develop historic contexts; evaluate properties for National Register eligibility; prepare historic architectural resources survey reports as part of environmental studies conducted by NCDOT and their consultants; assess effects of transportation projects on National Register-eligible and -listed properties; coordinate mitigation efforts between NCDOT, State Historic Preservation Office, and other concerned agencies and individuals.

### **North Carolina Department of Transportation**

**March 1994 to June 1997**

Division of Highways  
Planning and Environmental Branch  
Historic Architectural Resources Section  
Raleigh, North Carolina

#### Historic Restoration/Preservation Specialist I

Secure compliance with state and Federal historic preservation laws and regulations with respect to historic architectural properties; perform architectural field surveys; develop historic contexts; evaluate properties for National Register eligibility; prepare historic architectural resources survey reports as part of environmental studies conducted by NCDOT; assess effects of transportation projects on National Register-eligible and -listed properties; coordinate mitigation efforts between NCDOT, State Historic Preservation Office, and other concerned agencies and individuals.

### **Historic Preservation Division**

**June-August 1990**

Department of Planning and Development  
City of Dallas, Texas

#### Historic Preservation Intern

Performed architectural field surveys and background research in preparation for a local historic district nomination of an industrial section of Dallas, Texas; presented results to the Dallas Landmark Commission.

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## EDUCATION

### **University of Houston Law Center Houston, Texas**

*J.D. Candidate*

**1998-present**

### **University of Virginia Charlottesville, Virginia**

*Ph.D. Candidate, Architectural History*

Major Field: Early American Architecture

Minor Fields: Medieval Architecture; European Architecture after 1750

**1992-94**



*M.A. Architectural History*  
Major Field: Early American Architecture  
Thesis: George Washington's Mount Vernon as British Palladian Architecture

1991

**Texas A&M University**  
College Station, Texas

*B.A. History*  
Minor: Historic Preservation

1989

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## ARCHITECTURAL SURVEY REPORTS

<b>Phase II (Intensive)</b> NC 24 from 2.8 Miles East of I-95 to I-40 Cumberland, Sampson, and Duplin County, North Carolina TIP No. R-2303	July 1998
<b>Phase I (Reconnaissance)</b> Preliminary Identification: US 321/74 Bypass, Gastonia Thoroughfare Plan Gaston and Mecklenburg County, North Carolina	June 1998
<b>Phase II (Intensive)</b> Final Identification: Rocky Mount Thoroughfare Plan, Southern Study Area Nash and Edgecombe County, North Carolina	October 1997
<b>Phase II (Intensive)</b> Final Identification: Rocky Mount Thoroughfare Plan, Northern Study Area Nash and Edgecombe County, North Carolina	October 1997
<b>Phase I (Reconnaissance)</b> Preliminary Identification: Rocky Mount Thoroughfare Plan, Southern Study Area Nash and Edgecombe County, North Carolina	August 1997
<b>Phase I (Reconnaissance)</b> Preliminary Identification: Rocky Mount Thoroughfare Plan, Northern Study Area Nash and Edgecombe County, North Carolina	August 1997
<b>Phase II (Abridged)</b> US 64 from NC 45 East of Plymouth to SR 1235 East of Columbia Washington and Tyrrell County, North Carolina TIP No. R-2548	April 1997
<b>Phase II (Abridged)</b> NC 55 from SR 1108 (Wake Chapel Road) to SR 1114 (Ralph Stevens Road) Wake County, North Carolina TIP No. R-2907	February 1997
<b>Phase II (Intensive)</b> Final Identification: Elizabeth City Thoroughfare Plan Pasquotank County, North Carolina Multiple TIP No.'s	June 1996
<b>Phase I (Reconnaissance)</b> Elizabeth City Thoroughfare Plan Pasquotank County, North Carolina Multiple TIP No.'s	February 1996

<p><b>Phase II (Abridged)</b>  SR 1716 (Graham-Hopedale Road) from Providence Road in Graham to US 70  (Church Street) in Burlington  Alamance County, North Carolina  TIP No. U-2410</p>	<b>November 1995</b>
<p><b>Phase II (Abridged)</b>  NC 56 from I-85 at Butner to NC 50 South of Creedmoor  Granville County, North Carolina  TIP No. R-2542</p>	<b>August 1995</b>
<p><b>Phase II (Abridged)</b>  US 17 from the New Bern Bypass to the Proposed Washington Bypass  Craven and Beaufort County, North Carolina  TIP No. R-2513</p>	<b>June 1995</b>
<p><b>Phase II (Abridged)</b>  Addendum: NC 119 from I-85 to South of SR 1917  Alamance County, North Carolina  TIP No. U-3109</p>	<b>June 1995</b>
<p><b>Phase II (Abridged)</b>  NC 24-27 from East of SR 1963 to East of SR 1783  Albemarle, Stanly County, North Carolina  TIP No. R-2530A</p>	<b>May 1995</b>
<p><b>Phase II (Abridged)</b>  Widen NC 55 from US 17 in Bridgeton to NC 304 in Bayboro  Craven and Pamlico County, North Carolina  TIP No. R-2539</p>	<b>May 1995</b>
<p><b>Phase II (Abridged)</b>  Addendum: US 15-501 from the Proposed Pittsboro Bypass to the Chapel Hill Bypass  Chatham and Orange County, North Carolina  TIP No. R-942</p>	<b>April 1995</b>
<p><b>Phase II (Abridged)</b>  Addendum: US 15-501 from the Proposed Pittsboro Bypass to the Chapel Hill Bypass  Chatham and Orange County, North Carolina  TIP No. R-942</p>	<b>April 1995</b>
<p><b>Phase II (Abridged)</b>  NC 119 from I-85 to South of SR 1917  Alamance County, North Carolina  TIP No. U-3109</p>	<b>April 1995</b>
<p><b>Phase I (Reconnaissance)</b>  Cook Road (SR 1311) and Shallowford Church Road (SR 1301) Upgrade and Extension from  US 70 to NC 87  Alamance County, North Carolina  TIP No. U-3110</p>	<b>April 1995</b>
<p><b>Phase I (Reconnaissance)</b>  US 421-NC 87 Sanford Bypass  Lee County, North Carolina  TIP No. R-2417</p>	<b>April 1995</b>
<p><b>Phase I (Reconnaissance)</b>  US 17 from the New Bern Bypass at SR 1003 to the Washington Bypass near SR 1127  Craven and Beaufort County, North Carolina  TIP No. R-2513</p>	<b>April 1995</b>



<p><b>Phase II (Abridged)</b>  NC 24 from Swansboro to West of NC 58 at Cape Carteret  Onslow and Carteret County, North Carolina  TIP No. R-2105AA</p>	<p><b>March 1995</b></p>
<p><b>Phase II (Abridged)</b>  SR 1426 (Bolling Road)/Fifth Street from SR 1400 (W. Tenth St.) to NC 48 (Roanoke Avenue)  Roanoke Rapids, Halifax County, North Carolina  TIP No. U-1007</p>	<p><b>February 1995</b></p>
<p><b>Phase II (Abridged)</b>  NC 54 from SR 2106 to NC 119  Alamance County, North Carolina  TIP No. R-2538</p>	<p><b>January 1995</b></p>
<p><b>Phase II (Abridged)</b>  St. Mark's Church Road from SR 1146 (Kirkpatrick Road) to US 70  Alamance County, North Carolina  TIP No. U-2905</p>	<p><b>January 1995</b></p>
<p><b>Phase II (Intensive)</b>  Addendum: US 17 New Bern Bypass from the Jones-Craven County Line to SR 1438 near  Vanceboro  Craven County, North Carolina  TIP No. R-2301</p>	<p><b>December 1994</b></p>
<p><b>Phase II (Abridged)</b>  Addendum: US 15-501 from the Proposed Pittsboro Bypass to the Chapel Hill Bypass  Chatham and Orange County, North Carolina  TIP No. R-942</p>	<p><b>December 1994</b></p>
<p><b>Phase II (Abridged)</b>  US 64 from NC 45 East of Plymouth to 2000 Feet East of NC 32  Washington County, North Carolina  TIP No. R-2548</p>	<p><b>December 1994</b></p>
<p><b>Phase II (Abridged)</b>  US 17 from Trent Road (SR 1278) to US 70 Business/NC 55  New Bern, Craven County, North Carolina  TIP No. U-2556</p>	<p><b>November 1994</b></p>
<p><b>Phase II (Abridged)</b>  Addendum: US 15-501 from the Proposed Pittsboro Bypass to the Chapel Hill Bypass  Chatham and Orange County, North Carolina  TIP No. R-942</p>	<p><b>November 1994</b></p>
<p><b>Phase II (Abridged)</b>  NC 98 (Wake Forest Bypass) from West of SR 1923 to East of SR 2053  Wake County, North Carolina  TIP No. R-2809</p>	<p><b>October 1994</b></p>
<p><b>Phase II (Abridged)</b>  Construct Concord-Kannapolis Westside Bypass from NC 49 to I-85  Cabarrus County, North Carolina  TIP No. R-2246</p>	<p><b>September 1994</b></p>
<p><b>Phase II (Abridged)</b>  US 15-501 from the Proposed Pittsboro Bypass to the Chapel Hill Bypass  Chatham and Orange County, North Carolina  TIP No. R-942</p>	<p><b>September 1994</b></p>

<p><b>Phase II (Abridged)</b>  Widen NC 180 from SR 2200 to NC 2052  Shelby vicinity, Cleveland County, North Carolina  TIP No. U-2221</p>	<b>August 1994</b>
<p><b>Phase II (Abridged)</b>  Widen NC 111 from SR 1710 to US 70  Goldsboro, Wayne County, North Carolina  TIP No. U-2715</p>	<b>August 1994</b>
<p><b>Phase II (Abridged)</b>  Widen US 70 from SR 2851 (Penry Road) to the Proposed Greensboro Eastern Loop  Interchange  Guilford County, North Carolina  TIP No. U-2581A</p>	<b>August 1994</b>
<p><b>Phase I (Reconnaissance)</b>  Interchange Alternatives US 117 from US 13 in Goldsboro to Proposed US 264 Bypass  Wilson County, North Carolina  TIP No. R-1030D</p>	<b>August 1994</b>
<p><b>Phase I (Reconnaissance)</b>  Widen SR 2472 (Mallard Creek Church Road) from I-85 to US 29 and SR 2833 from US 29 to  NC 49  Mecklenburg County, North Carolina  TIP No. U-2508</p>	<b>August 1994</b>
<p><b>Phase II (Abridged)</b>  Widen NC 62 from US 158-NC 86 East of Yanceyville to NC 57 in Milton  Caswell County, North Carolina  TIP No. R-3103</p>	<b>July 1994</b>
<p><b>Phase II (Abridged)</b>  Maple Street Extension from I-85 to NC 87 at Moore Street  Graham, Alamance County, North Carolina  TIP No. U-2411</p>	<b>July 1994</b>
<p><b>Phase II (Abridged)</b>  US 17 Bypass  Elizabeth City, Pasquotank County, North Carolina  TIP No. R-2515</p>	<b>June 1994</b>
<p><b>Phase I (Reconnaissance)</b>  I-95 from 1.1 Miles South of US 158 to the Virginia State Line  Halifax and Northhampton County, North Carolina  TIP No. I-905</p>	<b>June 1994</b>
<p><b>Phase I (Reconnaissance)</b>  Glensford Road Extension from SR 1400 to SR 1404  Cumberland County, North Carolina  TIP No. U-3107</p>	<b>June 1994</b>
<p><b>Phase I (Reconnaissance)</b>  Construct Left Turn Lanes on US 1 on all Median Crossovers from 0.23 Mile North of US 15-501  to 0.15 Mile North of NC 78  Lee County, North Carolina  TIP No. W-2940</p>	<b>May 1994</b>
<p><b>Phase I (Reconnaissance)</b>  Widen US 70 from a Two-Lane Undivided Facility to a Five-Lane Curb and Gutter Facility  McDowell County, North Carolina  TIP No. R-3115</p>	<b>May 1994</b>



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## PUBLICATIONS & PRESENTATIONS

- Author** 1996  
"Romanticism and the Picturesque," in A Romantic Architect in Antebellum North Carolina: The Works of Alexander Jackson Davis by Edward T. Davis, et al. University of North Carolina Press. (forthcoming 1999)
- Writer and Assistant Editor** 1995-96  
Museum Exhibit and Catalogue, "A Romantic Architect in Antebellum North Carolina: The Works of Alexander Jackson Davis." The Historic Preservation Foundation of North Carolina, Inc. and the North Carolina Museum of History, Raleigh, North Carolina
- Lecturer** November 1993  
"Thomas Jefferson's Design for the University of Virginia"  
University of Virginia Student Exhibition on the Lawn  
Charlottesville, Virginia
- Lecturer** November 1992  
"Mount Vernon from Palladianism to Postmodernism"  
Virginia Museum of Fine Arts  
Richmond, Virginia
- Lecturer** October 1992  
"Mount Vernon from Palladianism to Postmodernism"  
Virginia Commonwealth University  
Richmond, Virginia
- Author** 1992  
Entry No. 53, "Gatekeepers' Lodges and Gate, Mount Vernon," in The Making of Virginia Architecture by Charles E. Brownell, et al. (Richmond: Virginia Museum of Fine Arts, 1992)
- Lecturer** November 1991  
"Mount Vernon and the Colonial Revival"  
4th Annual Architectural History Symposium  
University of Virginia  
Charlottesville, Virginia
- Lecturer**  
"Thomas Jefferson and the University of Virginia"  
American Institute of Architects' National Council on Design  
Charlottesville, Virginia April 1991
- 

## PROFESSIONAL MEMBERSHIPS

- Society of Architectural Historians 1989 to present