



North Carolina Department of Cultural Resources
State Historic Preservation Office

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Jeffrey J. Crow, Deputy Secretary
Office of Archives and History

Division of Historical Resources
David J. Olson, Director

June 26, 2002

MEMORANDUM

TO: William D. Gilmore, Manager
Project Development and Environmental Analysis Branch
Department of Transportation, Division of Highways

FROM: David Brook *for David Brook*

SUBJECT: Draft Historical Architectural Survey Report, Enola Road from I-40 to NC 18,
Morganton, U-2551, Burke County, ER 02-9941

Thank you for your letter of May 21, 2002, transmitting the draft survey report by Mattson, Alexander and Associates, Inc. concerning the above project. We regret that due to staff vacancies we were unable to respond in a timelier manner.

For purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that the following properties are eligible for listing in the National Register of Historic Places:

- BK0042* ➤ Broughton Hospital Historic District National Register amendment, including the Carpenter Building, Hooper Building, Moran Building, and the Staff Cottages south of I-40 under Criteria A, B and C. The district and boundary extension is significant in the areas of health care and architecture as well as for its association with Dr. Patrick Livingston Murphy.
- BK0068: Expansion*
- (11-14-01)* ➤ Dale's Market National Register amendment to include the Dale House under Criteria A and C as a rare illustration of a traditional commercial pattern in rural communities and as a fine example of a Craftsman style house.
- BK0069 (House)*
- Mull's Store and House under Criteria A and C as a good, but now rare, illustration of the traditional association of the owner's house to general store and a good example of roadside architecture.
- BK0067*

The following properties do not appear to be eligible for the National Register or to warrant further study:

Houses 4-23

We have kept one copy of the draft report as a reference and clearly marked its draft status. We will return this copy upon receipt of the final report.

	Location	Mailing Address	Telephone/Fax
Administration	507 N. Blount St, Raleigh, NC	4617 Mail Service Center, Raleigh 27699-4617	(919) 733-4763 • 733-8653
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Survey & Planning	515 N. Blount St, Raleigh, NC	4618 Mail Service Center, Raleigh 27699-4618	(919) 733-4763 • 715-4801

Page 2
William D. Gilmore
June 26, 2002

The above comments are made pursuant to Section 106 of National Historic Preservation Act and Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919/733-4763. In all future communication concerning this project, please cite the above referenced tracking number.

cc: Barbara Church, NCDOT
Matteson, Alexander & Associates

bc: Brown/McBride ✓
County

SUPPLEMENT

**PHASE II INTENSIVE HISTORICAL ARCHITECTURAL
SURVEY AND REPORT
ENOLA ROAD FROM I-40 TO NC 18
CITY OF MORGANTON
BURKE COUNTY
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
TIP NO. U-2551
STATE PROJECT NUMBER 8.285201
FEDERAL PROJECT NUMBER STP-1922(1)**

Prepared By

**Mattson, Alexander and Associates, Inc.
Charlotte, North Carolina 28205**

Prepared For

**Arcadis Geraghty and Miller, Inc.
Raleigh, North Carolina 27607**

22 July 2002

SUPPLEMENT

PHASE II INTENSIVE HISTORICAL ARCHITECTURAL
SURVEY AND REPORT
ENOLA ROAD IMPROVEMENTS
CITY OF MORGANTON
BURKE COUNTY
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
TIP NO. U-2551
STATE PROJECT NUMBER 8.285201
FEDERAL PROJECT NUMBER STP-1922(1)

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Frances Alexander 7-22-02
Principal Investigator Date
Mattson, Alexander and Associates, Inc.

Historic Architectural Resources Date
North Carolina Department of Transportation

MANAGEMENT SUMMARY

This North Carolina Department of Transportation project is entitled, *Enola Road From I-40 to N.C. 18, Burke County*. The T.I.P. Number is U-2551. The proposed action is an improvement to existing Enola Road-Old N.C. 18 (S.R. 1922-S.R. 1924), currently a two-lane, north-south facility in the City of Morganton in Burke County. The focus of the improvements is the approximately one-mile section of the road from I-40 (including the interchange) north to N.C. 18/South Sterling Street. The project area is approximately fifty feet on each side of the centerline of Enola Road-Old N.C. 18. As part of this action, N.C.D.O.T. also proposes upgrading the I-40/Enola Road interchange (**Figures 1 and 2**).

This architectural survey was conducted and the report prepared in order to identify historic architectural resources located within the area of potential effects (A.P.E.) as part of the environmental studies conducted by N.C.D.O.T. and documented by an environmental assessment (E.A.). This report is prepared as a technical addendum to the E.A., which is on file at the North Carolina Department of Transportation, Raleigh, North Carolina. The technical addendum is part of the documentation undertaken to comply with the National Environmental Policy Act (N.E.P.A.) and the National Historic Preservation Act of 1966, as amended. Federal regulations require federal agencies to take into account the effect of their undertakings on properties included in, or eligible for inclusion in, the National Register of Historic Places and to afford the Advisory Council on Historic Preservation a reasonable opportunity to comment on such undertakings.

The report meets the guidelines for architectural surveys established by N.C.D.O.T. (15 June 1994). These guidelines set forth the following goals for architectural surveys: (1) to determine the A.P.E. for the project; (2) to locate and identify all resources fifty years of age or older within the A.P.E.; and (3) to determine the potential eligibility of these resources for listing in the National Register of Historic Places. In addition, this report conforms to the expanded requirements for architectural survey reports developed by N.C.D.O.T. and the North Carolina Department of Cultural Resources (February 1996).

The methodology for the survey consisted of background research into the historical and architectural development of the area and a field survey of the A.P.E. (see **Figure 3** in **Appendix A**). The field survey was conducted to delineate the A.P.E. of the proposed highway construction and to identify all properties within this area that were built prior to 1952. The boundaries of the A.P.E. are shown on U.S. Geological Survey (U.S.G.S.) topographical maps. The A.P.E. comprises a variety of residential, commercial, institutional, and governmental properties. One hundred percent of the A.P.E. was surveyed (see **Figure 2**).

A total of 23 resources were identified as being at least fifty years of age. Three resources were evaluated in the Property Inventory and Evaluations Section of the report, including two currently listed in the National Register, the Broughton Hospital Historic District and the U. S. B. Dale's Market. The present study recommends National Register boundary expansions for both these properties to encompass additional contributing resources that lie within the A.P.E. One other property, Mull's Store and House, is also evaluated in this report and recommended as eligible for the National Register.

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Other Properties Evaluated and Considered Not Eligible for the National Register

(See Appendix A)

No. 4	House
No. 5	House
No. 6	House
No. 7	House
No. 8	House
No. 9	House
No. 10	House
No. 11	House
No. 12	House
No. 13	House
No. 14	House
No. 15	House
No. 16	House
No. 17	Willis House
No. 18	House
No. 19	House
No. 20	House
No. 21	House
No. 22	House
No. 23	House

BK0042 Broughton Hospital District
BK0068 Broughton Hospital District Boundary Expansion
BK0043 Dale's Market
BK0069 Dale's House
BK0067 Mull's Store and House

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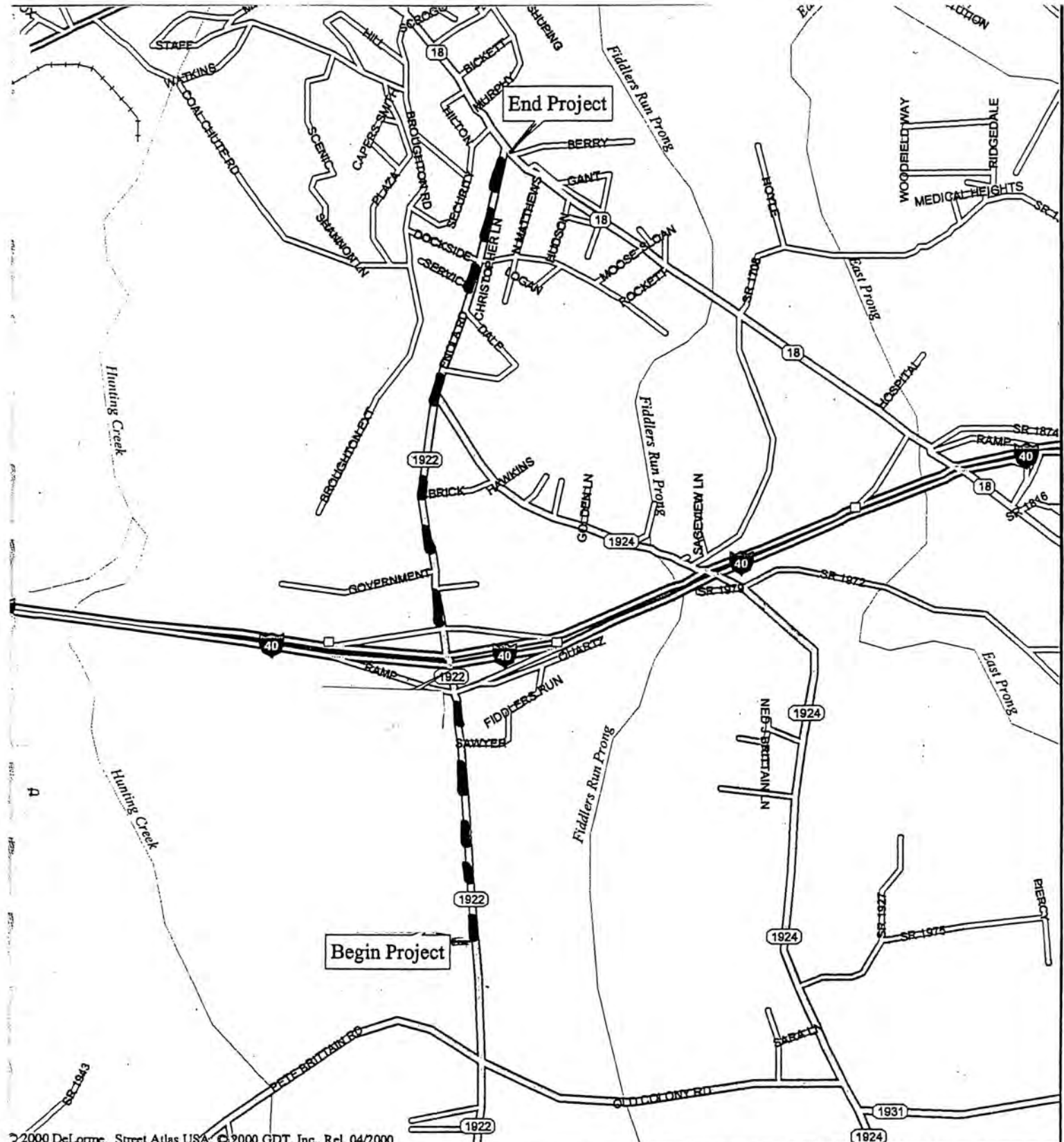
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II. INTRODUCTION

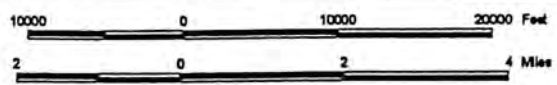
This Phase II intensive level architectural survey was undertaken in conjunction with the proposed improvements to Enola Road in the City of Morganton, Burke County. The project extends for approximately one mile along existing two-lane Enola Road-Old N.C. 18 (S.R. 1922-S.R. 1924), from the vicinity of the I-40 interchange northward to N.C. 18/South Sterling Street (**Figures 1 and 2**). The project area extends roughly fifty feet on each side of the centerline of Enola Road-Old N.C. 18. As part of this undertaking, N.C.D.O.T. also proposes upgrading the I-40/Enola Road interchange. The T.I.P. Number for this North Carolina Department of Transportation project is U-2551. Mattson, Alexander and Associates, Inc. of Charlotte, North Carolina, conducted this study for the North Carolina Department of Transportation, Raleigh, North Carolina. Richard L. Mattson and Frances P. Alexander served as the principal investigators, and the work was undertaken in November and December 2001.

This architectural survey was undertaken in accordance with the Department of Transportation Act of 1966, Section 106 of the National Historic Preservation Act of 1966, as amended (36 C.F.R. 800), and the F.H.W.A. Technical Advisory T 6640.8A (Guidance for Preparing and Processing Environmental and Section 4(f) Documents). Section 106 requires the identification of all properties eligible or potentially eligible for listing in the National Register of Historic Places according to criteria defined in 36 C.F.R. 60. In order to comply with these federal regulations, this survey followed guidelines set forth in Phase II Survey Procedures for Historic Architectural Resources (N.C.D.O.T., 15 June 1994) and expanded requirements for architectural survey reports developed by N.C.D.O.T. and the North Carolina Department of Cultural Resources (February 1996). Federal regulations require that the area of potential effects (A.P.E.) for the undertaking must be determined. The A.P.E. is defined as the geographical area, or areas, within which an undertaking may cause changes to the character or use of historic properties, if such potentially eligible properties exist. The A.P.E. is depicted on U.S. Geological Survey topographical maps (see **Figure 2**).

The A.P.E. is based primarily on the relationship of the proposed corridor to both natural and manmade boundaries. At the north end of the project, the A.P.E. is clearly marked by four-lane N.C. 18/South Sterling Street. Significantly widened and improved in recent years, this east-west roadway now includes modern commercial properties and open space in the vicinity of the project. At the south end of the project, the modern North Liberty School/Liberty Middle School complex defines the limits of the A.P.E. Along the west side of the project north of I-40, the A.P.E. extends into the Broughton Hospital Historic District (National Register 1986). The east side of the A.P.E. in this area encompasses a small number of dwellings along Enola Road-Old N.C. 18 built mostly between the 1920s and 1940s. Here, the boundary of the A.P.E. is marked by post-1950s residential streets to the east of Enola Road-Old NC 18. South of I-40, modern buildings and landscaping associated with the expansive Western Carolina Center campus define the west side of the A.P.E. The east side of the A.P.E. south of I-40 is characterized by rolling fields east of Enola Road. Rows of one story, frame cottages erected for Broughton Hospital staff quarters in the 1920s and 1930s, and now owned and occupied by the Western Carolina Center, line both sides of Enola Road south of I-40 and are included within the A.P.E.



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SCALE: 1" = 0.25 MILES



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VICINITY MAP
 ENOLA ROAD (FROM I-40 TO NC 18)
 T.I.P. PROJECT U-2551
 BURKE COUNTY, NORTH CAROLINA

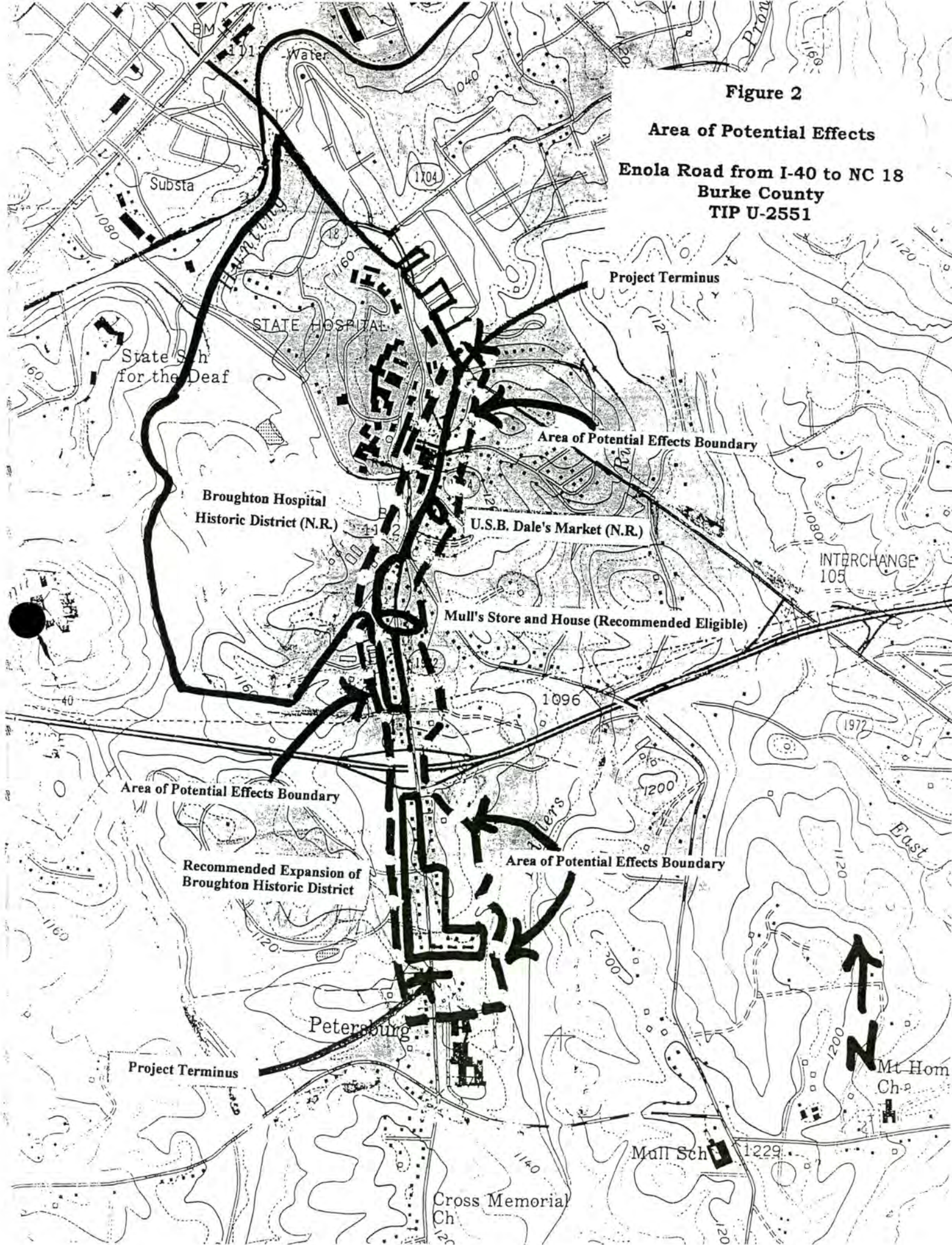
Figure No.

1

Figure 2

Area of Potential Effects

Enola Road from I-40 to NC 18
Burke County
TIP U-2551



III. PHYSICAL ENVIRONMENT

The project area is sited southeast of downtown Morganton in the foothills of the Blue Ridge Mountains. Established in the late eighteenth century, Morganton emerged as a significant trading and political center in western North Carolina during the nineteenth and early twentieth centuries. The grounds of major state institutions, including Broughton Hospital, the Western Carolina Center, the North Carolina School for the Deaf, and Western Piedmont Community College are located in and around the project area (**Figure 4**). Founded in 1875, Broughton Hospital, now encompassed within a National Register Historic District (1987), occupies rolling hills along the west side of the project north of I-40. Interstate Highway 40 cuts east-west through the A.P.E., and the I-40 interchange stands in the project area. This interchange has attracted a collection of modern commercial and office buildings, including a small office park just north of I-40 on the west side of Enola Road and a modern gasoline station and a shopping center on the east side of Enola Road south of the interstate.

South of I-40, the modern Western Carolina Center, a treatment center for the mentally disabled, is sited on a sprawling campus around the project area. Cottages now owned and occupied by the center, but erected in the 1920s as Broughton Hospital staff quarters, line both sides of Enola Road south of I-40. The historic North Carolina School for the Deaf, which is also a National Register historic district, and the modern community college stand immediately to the west of the Broughton Hospital grounds and outside the A.P.E. Just east of Broughton Hospital within the A.P.E. are several small neighborhoods that reflect the subdivision of middling farms into residential tracts in the early and mid-twentieth centuries. While modest residences and several commercial buildings erected between 1900 and World War II line Enola Road-Old N.C. 18 in the project area, dwellings erected in recent decades dominate these neighborhoods. Notable among the early twentieth century buildings in this area is the historic U. S. B. Dale's Market (National Register 1987), which faces Enola Road-Old N.C. 18 opposite the Broughton Hospital Historic District.

The northern terminus of the project is NC 18/South Sterling Street. This east-west corridor has been widened and improved in recent years, and now includes some modern commercial buildings. The south end of the project contains the former Broughton Hospital staff cottages that are now part of the Western Carolina Center. A modern school complex and a small crossroads community at the junction of Enola Road and S.R. 1940 are located just south of the project and the A.P.E.

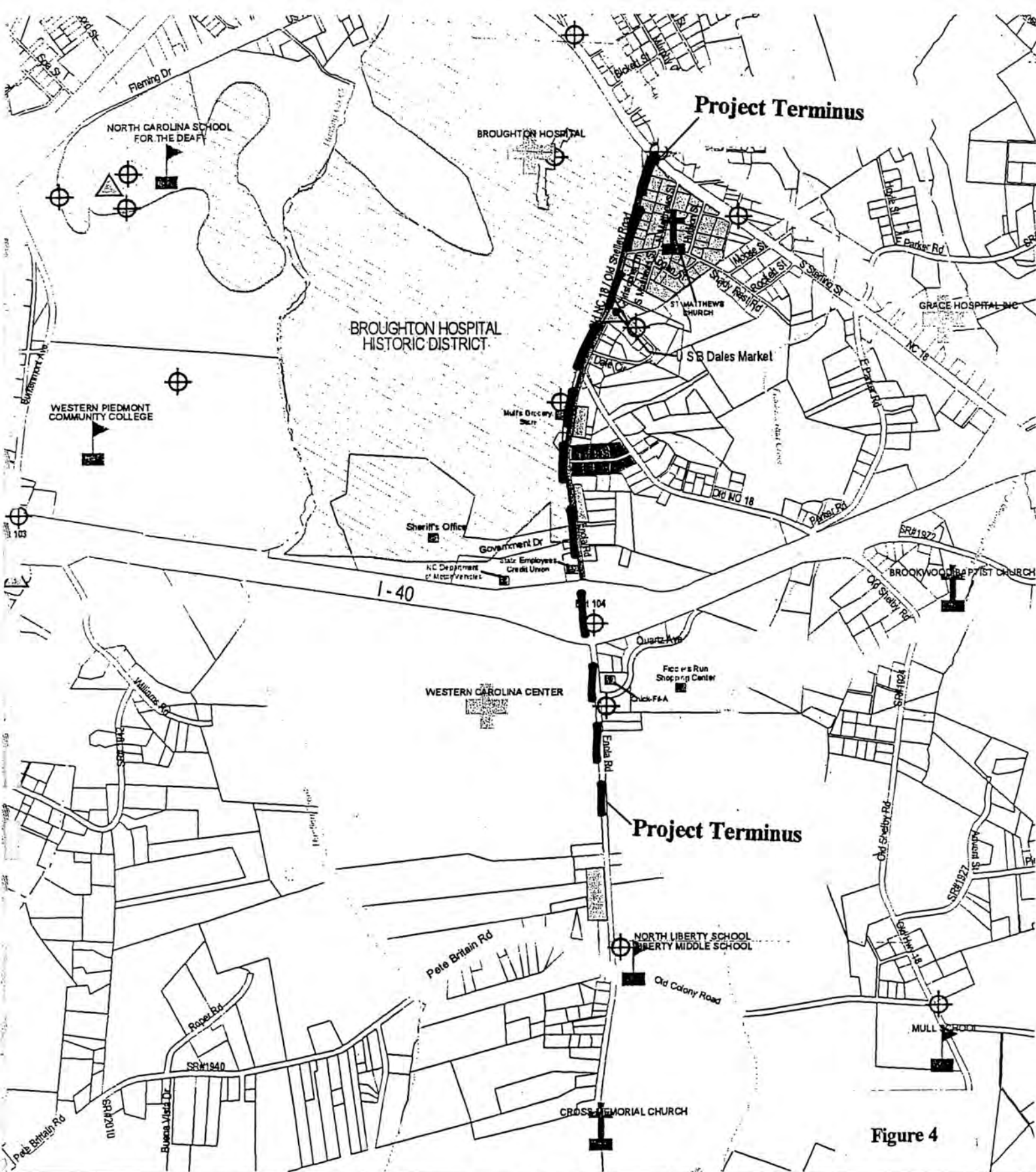


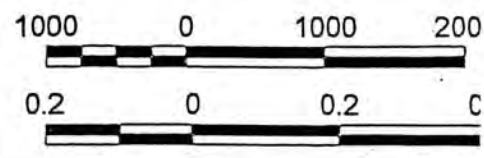
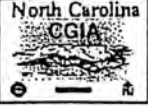
Figure 4



Prepared For:



Sources:



SCALE: 1"= 1000'

IV. METHODOLOGY

This Phase II architectural survey was conducted as part of the planning for the Enola Road improvements project in the City of Morganton, Burke County. The architectural survey for this federally funded project was undertaken in accordance with the Department of Transportation Act of 1966, Section 106 of the National Historic Preservation Act of 1966, as amended (36 C.F.R. 800), and the F.H.W.A. Technical Advisory T 6640.8A (Guidance for Preparing and Processing Environmental and Section 4(f) Documents). The survey followed guidelines set forth in *Phase II Survey Procedures for Historic Architectural Resources* (N.C.D.O.T., 15 June 1994).

The survey was conducted with the following goals: 1) to determine the area of potential effects (A.P.E.), which is defined as the geographic area or areas within which a project may cause changes to the character or use of historic properties, if any such properties exist; 2) to identify all resources at least fifty years of age within the A.P.E.; and 3) to evaluate these resources according to National Register of Historic Places criteria. The geographical context for evaluating architectural resources was the City of Morganton. The field survey was conducted in November and December 2001 to delineate the A.P.E. and identify all resources within the A.P.E. that appear to have been built before 1952. One hundred percent of the A.P.E. was surveyed.

The background research included the analysis of an assortment of primary and secondary sources. The publication, *A Guide to the Historic Architecture of Western North Carolina* (Bishir et al. 1999) offered an historical and architectural overview of Morganton and Burke County. The architectural survey files at the Western Office of the Division of Archives and History in Asheville were searched for previously recorded properties. The staffs of the City of Morganton Planning Department, Broughton Hospital, Western Piedmont Community College, and the Historic Burke Foundation in Morganton were interviewed concerning historic resources in the study area. In 1983-1984, Burke County and the City of Morganton jointly sponsored a comprehensive architectural inventory of the county that culminated in the publication, *Historic Burke: An Architectural Inventory of Burke County* (Cotton 1987). In 1986, the *Morganton (North Carolina) Multiple Resource Area National Register Nomination* was completed. This nomination contained nine historic districts and five individual properties, including the Broughton Hospital Historic District and U.S.B. Dale's Market within the project area (Wylie 1986). All of these sources provided information regarding the history of the area and historic properties in and around the A.P.E.

Following the research and a preliminary field survey that identified all resources at least fifty years of age within the A.P.E., a preliminary presentation of findings was submitted to N.C.D.O.T. for review. The principal investigators subsequently conducted an intensive level field survey of those resources that were determined to merit intensive evaluation. For each of these resources the following information and supporting materials were provided: physical description and evaluation of integrity; photographs of the exterior and interior (with owner permission); site plan; historical background information; and a North Carolina Historic Structure Data Sheet. In addition, for those resources considered eligible for the National Register, the proposed boundaries were depicted on property maps.

V. HISTORICAL BACKGROUND ESSAY

Morganton is located in Burke County in the foothills of western North Carolina. The county was established in 1777, and in 1782 Morganton was platted near the center of the new county as the political seat. By the early nineteenth century, the town had developed into a regional political and trading center. A substantial planter class flourished in the Catawba River valley around Morganton, and the wealth and political influence of these prominent families contributed to the town's antebellum prosperity. In the 1830s, a stylish courthouse featuring a Baroque cupola and bold Doric columns arose on the large central square. By the onset of the Civil War, the town boasted a host of prominent and sophisticated Greek Revival houses, including the Cedars, renovated and enlarged in the 1870s, the Gaither House, and Creekside (Ervin 1896: 37; Wylie 1986: 8: 16-18; Bishir et al. 1999: 148-155).

The town's progressive leadership campaigned early on for transportation improvements, especially railroads to link Morganton with regional and national markets. The Western North Carolina Railroad reached Morganton's outskirts in 1861, and by 1880 extended westward through Morganton to Asheville in the Blue Ridge Mountains. In the late nineteenth and early twentieth centuries, Morganton grew and prospered. Between 1880 and 1890, the population doubled from around 800 to 1,600, and by 1900 approached 2,000. During the 1890s, streets were widened and macadamized, brick sidewalks laid, and electricity installed. By the turn of the twentieth century, cotton mills, furniture factories, tanneries, and woodworking shops lined the railroad corridor, while the business district expanded around the courthouse square. West Union and North Sterling streets emerged as the major commercial avenues. After a fire destroyed most of the frame commercial buildings in 1893, the downtown was rapidly rebuilt employing locally made brick. Reporting on this transformation, the local newspaper exclaimed, "Every brick at the Catawba River brick yards has already been purchased, and there are orders running on full time during the next season. Workmen of all kinds will be kept busy during the year and Morganton will look like another town before next winter sets in" (Phifer 1977: 318-334; Wylie 1986: 8: 12; Cotton 1987: 38-39; Bishir et al. 1999: 148-149).

In the late nineteenth century, Morganton also attracted two major new state institutions that became important sources of employment and spurred economic growth. Local resident Samuel McDowell Tate, former president of the Western North Carolina Railroad and a state legislator, was influential in winning for Morganton both the Western North Carolina Insane Asylum (now Broughton Hospital) and the North Carolina School for the Deaf. The two institutions arose on adjacent hilltops on the south side of Morganton and became landmarks to progressive national trends in health care (Phifer 1977: 130-131; Wylie 1986: 28-30; Bishir et al. 1999: 148).

In 1875, the state legislature authorized the Western North Carolina Insane Asylum to alleviate overcrowding in the state asylum in Raleigh. The new facility's foothills location was also considered a convenient site to serve patients in the western part of the state. The architect was Samuel Sloan of Philadelphia, widely known for his hospital and church architecture, including antebellum work in Wilmington, North Carolina. Sloan died in 1884, and the hospital was completed in 1886 by his assistant, Augustus Bauer. Sloan's overall design for the hospital complex closely followed the progressive theories of Dr. Thomas Story Kirkbride. Dr. Kirkbride had gained a national reputation as a reformer in the health care movement after the Civil War. Consistent with Kirkbride's ideas, the asylum included a spacious campus that allowed for pleasure gardens and therapeutic recreation, fireproof building materials, gas lighting, and small wards organized around centrally located offices. The chosen location was in proximity to both town and railroad, and contained agricultural fields to support patients and staff. Sloan and Bauer's main hospital building (Center Building), a magnificent red brick, domed structure

heavily embellished with picturesque elements, included an administrative core flanked by wings that housed rooms for patients. Originally, the north wing was for the males and the south wing for the females. In the early twentieth century, reform-minded hospital superintendent, Dr. Patrick Livingston Murphy, launched the Colony Program that included smaller, more intimate facilities to house and treat patients (Phifer 1977: 139-149; Wylie 1986: 8: 19, 40-43; Bishir et al. 1999: 153-154).

Listed in the National Register (1987), the Broughton Hospital remains in operation. The historic district includes the imposing Center Building as well as a host of later contributing structures. During the 1920s and 1930s, a series of red brick Colonial Revival buildings were constructed on the campus near the main hospital, rows of one story, frame staff houses appeared along Enola Road along the edge of the grounds, and a number of farm buildings were constructed near fields to the east and south of the main complex. In 1959, the state renamed the institution Broughton Hospital for former North Carolina governor, Melville Broughton (Wylie 1986: 40-41).

Sited west of Broughton Hospital, the North Carolina School for the Deaf opened its doors in 1894. To secure the school, Morganton donated the land and provided funds. Augustus Bauer was the architect for the Main Building, a striking four story, Queen Anne design with an E-shaped design and a central clock tower. Listed in the National Register (1987), the school retains the monumental Main Building as well as later red brick Colonial Revival and Romanesque Revival structures, some staff quarters, and farm buildings, notably a large gambrel roofed dairy barn near fields on the east side of the campus (Wylie 1986: 8: 21).

Stimulated by these state institutions, Morganton expanded rapidly during the first decades of the twentieth century. The town's population approached 3,000 by 1920 and then doubled to 6,000 by the Depression. During the 1920s, civic boosters launched a campaign that toasted Morganton as "Just the Right Town" for business and industry. This campaign highlighted the town's temperate climate, genial and hardworking citizenry, and proximity to raw materials. A new high school, Avery Avenue School, and a library were built in 1923, and large new hotels and banks enhanced the burgeoning business district (Phifer 1977: 227, 236-238, 240-244; Wylie 1986: 8: 7-9).

During these decades of prosperity, neighborhoods for wealthy residents and the growing middle class appeared around the central square. Today, National Register historic districts around Avery Avenue, Valdese Avenue, South King Street, North Green Street, and West Union Street represent the town's residential development in the late nineteenth and early twentieth centuries. As a whole, they contain a notable collection of Queen Anne and Colonial Revival residences, bungalows, and other historical revival dwellings (Wylie 1986: 8: 16-24).

Economic progress in Morganton and throughout the county was stimulated in this period by the passing of the state's Highway Act in 1921. Between the 1920s and World War II, new roads and bridges linked the county seat to an emerging statewide system of hard-surfaced highways. State funds were used to improve north-south N.C. 18 between Morganton and Shelby, North Carolina to the south. During the 1920s, the Dale family subdivided land facing N.C. 18 within the A.P. E. for single family dwellings. In the late 1920s, the Mull family, whose members had been merchants in Morganton since its founding, constructed a new grocery store on the state highway adjacent to Broughton Hospital (Wylie 1986: 8: 29; Yount Interview 2001).

While Morganton's development stalled in the early 1930s, by mid-decade, there were signs of recovery. Alpine Cotton Mills, which had closed in 1931, reopened in 1935, and in 1938 two additional mills appeared. Broughton Hospital and the School for the Deaf remained steady

employers, and many of the town's largest furniture and textile plants did not shut down, but temporarily cut back production and subsequently prospered. Architecturally, the 1930s witnessed many new Colonial Revival dwellings along West Union Street and Riverside Drive, and the building of stylish Art Deco commercial facades downtown. In addition, federally funded relief agencies, including the Works Progress Administration, built the Georgian Revival Morganton Community House, the Neo-Classical Revival Morganton Post Office, the Armory, and a collection of service buildings at Broughton Hospital (Phifer 1977: 351-353; Wylie 1986: 8: 14-15, 27).

The town's manufacturing concerns flourished during World War II, as industries converted to war-time production. Textile mills operated at nearly full capacity making fabrics for tents and uniforms, while furniture factories produced tent pegs, desks, and even plywood planes. The Great Lakes Carbon Company was established in 1943 as a defense industry, and began a trend of chemical industrial growth in the area that continues to the present (Phifer 1977: 353-358).

While manufacturing continues to play an integral part in the local economy, the town's principal employer is the state. Today, the south side of town includes not only the Broughton Hospital and the North Carolina School for the Deaf, but also large grounds for the state-funded Western Piedmont Community College and the Western Carolina Center, a sprawling, modern health-care complex for the mentally disabled. The historic campuses of Broughton Hospital and the School for the Deaf remain well-preserved landmarks to social history and humanitarian efforts in North Carolina. To the north, the historical center of Morganton is also substantially intact, featuring a range of building types that illustrate major architectural trends from the 1880s to the Depression. The 1986 National Register Multiple Resources Documentation Form for Morganton concludes, "The town as a whole, aside from modern development away from the historic core, is an outstanding example of the preservation of late nineteenth and early twentieth century commercial and residential architecture and their relationships to one another" (Wylie 1986: 7: 5).

Historic Context: Rural Stores in Burke County, ca. 1900 to 1952

Throughout much of its history, Burke County has been an agrarian county of self-sufficient farmers, who, until well after the Civil War, were generally removed from the major commercial centers of the state. Burke County farmers, as with other farm families of western North Carolina, produced what they needed for a comfortable subsistence. Because of this long history of self-sufficiency, commercial activities were generally limited to the county seat of Morganton until after the railroad was completed in the postbellum period. Morganton, which has served as the commercial as well as the governmental center of Burke County since its establishment in 1784, also served a larger regional role well into the nineteenth century as the final point of trade for western settlers and explorers. Despite its role as entrepot, Morganton remained a small village through the first half of the nineteenth century with an estimated population of only 600-700 in 1860. The town was organized with a courthouse square as its focal point, and even in mid-century there were only a few frame stores and professional offices clustered along Union Street, then the main commercial row of town. Typical of the period, local merchants lived either in upper story apartments above ground level stores or near their businesses as local storekeeper William Mull did, and professionals, such as lawyer Tod Caldwell, commonly practiced from offices in their homes. (Wylie, *Historic and Architectural Resources of Morganton*, 1986: 8:28-8:31).

Although the long anticipated Western North Carolina Railroad was finished to within three miles of town on the eve of the Civil War, all rail construction was halted until 1867 when Morganton became the railroad's western terminus. The railroad opened the county to regular and reliable trade with cities outside the region. Farm products could be sent to urban markets, and the trains could return with mass-produced goods. After suffering heavy economic losses from the war, local commerce began to rally by 1869 as several specialty stores, including two jewelers and three confectioners, opened to complement the general mercantile establishments, hotels, and grist mills that formed the commercial base of Morganton. Among these merchants were members of the pioneering Mull and Pearson families, who had operated stores in Morganton since the establishment of the town. However, Morganton received its greatest boost in 1878 when the town was selected as the site for a new state-supported asylum. Construction began on the Western North Carolina Insane Asylum (now Broughton Hospital within the Broughton Hospital National Register Historic District), and immediately the institution did much to stimulate commercial growth, drawing professionals and laborers who demanded housing, goods, and services. The hospital opened in 1883, and the following year Morganton could boast of its 900 residents, thirty-four stores, four livery stables, three jewelers, two druggists, four hotels, as well as twelve grist mills and three saw mills. A second public institution, the North Carolina School for the Deaf, was constructed in 1894, further expanding the economic base of the town (Wylie, *Morganton*, 1986: 8:29)

Commercial development was not limited just to Morganton during the late nineteenth century. In the rural areas of Burke County, depots were built along the east-west rail line, creating a string of small villages most of which included a general store to serve neighboring farms. Other crossroad communities emerged near grist or saw mills, general stores, or after the Civil War the post offices being established as part of a national effort to improve mail service. Although Morganton remained the only sizable town in Burke County, by the 1890s there were as many as thirty post offices in Burke County, a testament to the growth of these country hamlets (Cotton 1987: 36-37).

One of these rural communities developed within the A.P.E. along Enola Road, south of Morganton near Broughton Hospital. The institution, with its large staff, created a demand for

nearby housing and neighborhood stores, and at the turn of the century, Grant Dale erected a one story, brick store across Enola Road from the new Broughton campus. With its masonry exterior and decorative brickwork and cast iron pilasters, **U.S.B. Dale's Market** (N.R. 1986) was more sophisticated than most crossroads general stores. The relative sophistication of Dale's Market was undoubtedly the result of all the new commercial construction occurring nearby in downtown Morganton during the 1890s. Before this time, the commercial district of Morganton (outside the A.P.E.) was still comprised primarily of simple, frame buildings with the notable exception of the brick Piedmont Bank (ca. 1889). This pattern soon changed when in 1893 a fire broke out in the Hunt House Hotel situated on the corner of Union and Sterling streets. Most of the commercial district was destroyed in the fire, and reconstruction began immediately under a new city ordinance requiring that all commercial building be constructed of brick with metal roofs. (The brick yards which had produced the building materials needed for the new state institutions made such an ordinance possible.) Following national trends, many of the new stores were substantial buildings designed in nationally popular styles, a testament to the growing prosperity of the town (Wylie, *Morganton*, 1986: 8:21; Wylie, *Dale's Market*, 1986: 8:1).

Into the early twentieth century, country stores were typically simple, frame, gable-front buildings with deep, narrow, one or two story forms, broad porches, and simple detailing. Before the introduction of truck shipments, the stores had to have large, open interiors to accommodate the ample storage room needed to maintain the large inventories held between infrequent rail deliveries. In its two story form, the rural general store often included a second story owner's apartment or storage room, and in Burke County, as throughout the state, the two story form was more common before the automobile age of the post-World War I era. Illustrative of this type is the two story, frame store (ca. 1890) of William Connelly in the small rail hamlet of Connelly Springs, east of Morganton, while the frame store and post office operated by John Mull at Camp Creek was typical of the one story version and far simpler in detailing than Dale's Market (Cotton 1987: 36-37; Wylie, *Dale's Market* 1986).

The heyday of the rural community in Burke County did not last long into the twentieth century. Although other small towns, particularly those such as Valdese and Drexel along the railroad, grew with industrialization, a number of farming communities were soon undercut by the introduction of rural free delivery (1903 in Burke County). This system of mail delivery centralized the once dispersed pattern of crossroads post offices, ending one of the primary functions of such communities. However, the true transformation of rural life came with the introduction of automotive travel and the subsequent highway and bridge construction campaigns. The exponential growth in automobile ownership by World War I created a sea change in the patterns of rural commerce. The growth of automobile ownership and highway improvements gave rural residents greater mobility which, in turn, altered shopping habits. Larger towns could offer a wider variety of goods and often lower prices, and the older general stores in rural locations soon became obsolete as the suppliers of all-purpose goods for local farmers. By installing gasoline pumps and adding hydraulic lifts for minor auto repairs, many rural merchants, such as U.S.B. Dale who added gasoline pumps at his store on Enola Road, tried to retain a customer base by catering to the motoring trade. New, smaller roadside stores, precursors to modern convenience stores, sprang up to provide a limited array of everyday goods and services. The smaller size of these stores reflected both their more limited commercial function and a new ease in restocking made possible by frequent truck shipments (Davis 1992; Bishir et al. 1999: 62-63).

The state had responded to the new demand for better roads with the passage of the Highway Act of 1921, and between the 1920s and World War II, a coordinated, statewide system of paved highways was constructed to link the major cities and county seats of the state. N.C. 18 was built

along north-south Enola Road to link Morganton with Shelby, the seat of Cleveland County, and from there to towns in South Carolina (Cotton 1987: 37). Within the A.P.E., two local merchant families responded to the new highway construction. With the designation of N.C. 18 as a state highway in the 1920s, the Dale family subdivided some of their land in the Enola community for single family dwellings, and in the late 1920s, the Mull family, whose members had been merchants in Morganton since its founding, constructed a grocery and feed store just south of Broughton on the new highway. Unlike the earlier Dale's Market which had no parking, the Mull Store was set back from Enola Road enough to allow for a row of parking in front of the store. Mull's Store also reflected developments in building materials. Their choice of cast stone, a concrete product fabricated to look like rock-faced stone, had become popular by the 1920s as other forms of concrete construction came into common use. The new material proved popular for commercial use, in particular, because it was easy to construct and was largely fireproof. Several years later, Paul Mull and his wife built their one story, fieldstone cottage directly across N.C. 18 from the store. The **Mull Store and House**, is recommended for National Register eligibility under Criterion A for commerce and under Criterion C for architecture (Wylie 1986: 8: 29; Yount Interview 2001).

Although the owners of Dale's Market and Mull's Store responded quickly to changes in transportation and new market conditions, both merchants, by living adjacent to or near their stores, illustrated a traditional pattern of rural commercial development. Dale's Market was subdivided from a larger tract of land on which the Dale family built an imposing, side gable bungalow (ca. 1925), its hilltop setting overlooking the back of the store. Just to the south on the east and west sides of Enola Road are the Mull Store and House. Several other examples of this pattern have survived in Burke County including the ca. 1900 Leonhardt House and Store on the outskirts of Morganton towards Valdese. The imposing Queen Anne Leonhardt House sits on a hill across the highway from H.D. Leonhardt's small, brick store with its distinctive metal-clad, stepped parapet. Despite the decline in country stores through the twentieth century, a few with an adjacent owner's house continued to be built. Located on S.R. 1129 in Silver Creek is Reep's Grocery, built in 1947, as a late illustration of this pattern. The unusual saddle-notched log grocery store and gasoline station was erected on Dysartsville Road, and the Reeps' house, similar in size, massing, and log construction, stands just to the north (Cotton 1987: 185).

Prior to the development of the automobile, this geographical association of owner's house and store was ubiquitous, but with automotive travel, merchants, as well as customers, were freed from their rural isolation. Commerce tended to centralize in larger towns or cities where a wider variety of goods and services were available, often for lower prices. Throughout the twentieth century, the trend away from dispersed rural general stores and towards commercial centralization within larger towns has increased. Interstate highway construction and overall road improvements have made rural stores increasingly obsolete, and such mercantile operations, either individually or as part of a rural store and house complex, have not survived in great numbers. Near the project area, New N.C. 18 is lined with retail and service-related enterprises, particularly near its junction with Interstate 40. A modern shopping center and convenience store have been built in recent years along Enola Road just south of the Interstate 40 interchange. Furthermore, the farming population, which once traded at local stores, has steadily declined while farmland has given way to modern development, particularly in Burke County where the number of farms has declined at a faster rate than in most other North Carolina counties (Bishir et al. 1999: 82; Cotton 1987: 35).

Although no comprehensive inventory of rural stores in Burke County has been undertaken, it is acknowledged that such buildings throughout the state's mountain region have "fallen to changing times" (Bishir et al. 1999: 209; see, too, Martin 1995: F: 124-127). The published

1987 architectural history of Burke County, *Historic Burke: An Architectural Inventory of Burke County, North Carolina*, identifies fewer than ten rural stores in its compilation of historic buildings (Cotton 1987: 120-185). While not complete, this list indicates the rarity of this once common rural resource. In addition to Dale's Market, the Mull Store, H.D. Leonhardt's Store, Connelly's Store, Mull's Store at Camp Creek, and Reep's Grocery, few rural stores date from the early twentieth century. Of those remaining are the Tilley Store in Smokey Creek, with its characteristic simple, frame, gable-front form, and the company store for Henry Mill Village. Although built as part of a planned mill community in eastern Burke County, the company store is a fine example. The substantial, two story, brick edifice has a deep, rectangular form, flat parapet, segmental arched, paired windows, and large storefront display windows (Cotton 1987: 36-37, 158).

As rural commerce declined, the downtown commercial district of Morganton expanded during the interwar years as farmers began driving into town for purchases. A number of new stores opened in the early 1920s, including department stores, hardware concerns, drug stores, and a book store. This pattern of steady, if not greatly expanding, trade continued through the interwar period, and Morganton did relatively well during the Depression of the 1930s with its banks and industries surviving. After World War II, the commercial base of the town continued to supply local residents and the surrounding region while the local textile and furniture industries boomed (Wylie, *Morganton*, 1986: 8:31). In the past two decades, however, strip commercial development, particularly near the I-40 interchanges have undercut some of downtown Morganton's once central commercial role.

Guidelines for Evaluating the Eligibility of Rural Stores in Burke County, ca. 1900-1952

Rural commercial properties dating from the turn of the twentieth century to 1952 in Burke County must survive substantially intact in order to be recommended as eligible for the National Register. For eligibility under Criterion A for commerce, country stores must retain their roadside locations. The properties must clearly illustrate the emergence of roadside commerce during the early twentieth century when stores were built along highways to serve the new automotive traffic. To be eligible under Criterion C for architecture, commercial buildings must have sufficient integrity to illustrate clearly the forms, styles, materials, and interior plans of their original designs. Eligible stores may have remodeled display windows or replacement doors, both of which are commonplace, but these alterations should conform to the original openings.

V. PROPERTY INVENTORY AND EVALUATIONS

Summary of Findings

A total of 23 resources within the A.P.E. were identified as being at least fifty years of age. Three resources were evaluated in the Property Inventory and Evaluations Section of the report, including two currently listed in the National Register, the Broughton Hospital Historic District (B.H.H.D.) and U.S.B. Dale's Market. The present study recommends National Register boundary expansions for both these properties to encompass additional contributing resources that are sited within the A.P.E. The proposed expansion of B.H.H.D. creates a discontinuous historic district that includes former staff cottages south of I-40. In addition to these two National Register properties, one other resource, Mull's Store and House, is also evaluated in this report and recommended as eligible for the National Register.

<u>Properties Listed in the National Register</u>		<u>Page No.</u>
No. 1	Broughton Hospital Historic District (1987)	21
No. 2	U.S.B. Dale's Market (1987)	43

Properties Evaluated Intensively and Considered Eligible for the National Register

No. 3	Mull's Store and House	52
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Other Properties Evaluated and Considered Not Eligible for the National Register (see Appendix A)

National Register Properties

No 1. Broughton Hospital Historic District (N.R. 1987)

Bounded by Enola Road, Highway 18, Bickett Street, and S. Sterling Street
Morganton, Burke County

Constructed between ca. 1878 and ca. 1940, the 337-acre Broughton Hospital Historic District (B.H.H.D.) contains the main building (Center Building), wards, staff houses, a cemetery, farm buildings and fields, and other support buildings associated with the facility (**Figures 5-7**). The hospital is noteworthy for its impressive architecture that features the work of prominent Philadelphia architect, Samuel Sloan. The hospital is also important for its innovative plan, including a connecting system of buildings with a central core for offices and administration, small wards, a separate infirmary, and an experimental farm. According to the nomination, the district is significant in the areas of health care and architecture (Criteria A and C), as well as for its association with Dr. Patrick Livingston Murphy, the hospital's first administrator and a national leader in the field of mental health (Criterion B).

Transfer of Property from Broughton Hospital to Western Piedmont Community College

On September 26, 2000, the North Carolina General Assembly transferred approximately 200 acres of land from the Broughton Hospital campus to Western Piedmont Community College (see **Figure 15** in **Appendix B**). Located outside the A.P.E., this tract included a sizable portion of the 337-acre Broughton Hospital Historic District south of the main complex of historic buildings. The tract is primarily former pastureland, but includes some farm outbuildings and other buildings that contribute to the historic district. The college transferred twelve of those acres located on the south side of the B.H.H.D. to Burke County. The county will use this land for an agricultural extension center office, which is currently under construction. This new center will be accessed from Government Drive via Enola Road, near the I-40/Enola Road interchange (Corley Interview 2001).

The transfer of land within the B.H.H.D. and the construction of the Agricultural Extension Center do not affect the boundaries or the eligibility of the B.H.H.D. Although a sizable portion of the grounds within the B.H.H.D. is now under the supervision of Western Piedmont Community College rather than Broughton Hospital, the historic district remains eligible for the National Register under the nominated criteria. The hospital campus retains its architectural integrity and associated fields and continues to illustrate significant developments in health care during late nineteenth and early twentieth centuries. The new agricultural extension center is a non-contributing resource within the historic district.

Proposed Changes to the Broughton Hospital Historic District Boundaries (Plates 1-26)

It is recommended that the B.H.H.D. be expanded eastward to Enola Road to encompass four contributing resources on the Broughton Hospital campus excluded from the 1987 boundaries (**Figures 6-7**) (**Plates 1-12**). It is also recommended that the boundary be expanded southward to include rows of former hospital staff cottages south of I-40. Because modern development and I-40 now clearly separate these cottages from the existing B.H.H.D., this southward expansion creates a discontinuous historic district (**Figures 8-9**) (**Plates 13-26**).

The recommended eastward expansion of the B.H.H.D. includes the Carpenter Building (1950), the adjacent Hooper Building (1952), a former hospital staff cottage (1920s), which are all on Enola Road, and the Moran Building (1948) on N.C. 18/South Sterling Street. The Carpenter, Hooper, and Moran buildings did not meet the fifty-year age requirement in 1987 and were therefore drawn out of the district. The staff cottage, located just to the north of the Hooper

Building, was also excluded. The 1950 Carpenter Building, originally the Nurses Dormitory, is a four story, L-shaped brick building with a flat roof and limited detailing. The building has original steel sash windows and a flat-roofed canopy on the main façade. The austerity of the exterior is broken by the burnished steel surrounds that enframe the main doors on the front and rear elevations. The interior has its original plan, including front and rear lobbies and transverse hallways. The original linoleum flooring and hollow core, wooden doors also remain intact. The building is currently used for administrative offices. The 1952 Hooper Building is a spacious, functional one story, brick warehouse with a flat roof and steel sash windows. The structure has original truck loading docks on the side elevations. The building was erected for storing hospital supplies, including foodstuffs, and contains original cold storage lockers and concrete flooring. It continues to function as a warehouse for the hospital. The 1948 Moran Building, originally the Male Attendants' Quarters, is a utilitarian, two story, brick building with a flat roof and steel sash windows. A two story, glazed stair tower with steel framing connects the building's two main units facing Enola Road. Built as a dormitory for male hospital attendants, this building later was used by the Broughton Hospital fire department. It now contains administrative offices and the hospital police department. The original plan, hollow core doors, and linoleum flooring remain substantially intact. Finally, the well preserved, 1920s staff cottage typifies other cottages erected along Enola Road for hospital workers in this period. The dwelling is a one story, three bay, double pile, side gable dwelling with two rear wings, six-over-six windows, and a side porch.

This study also recommends that the B.H.H.D. be expanded to include the former staff cottages along Enola Road south of I-40. Thirty-five such cottages form a clearly defined district in this area. Thirty-three are considered contributing resources and only two cottages are non-contributing. Historically associated with Broughton Hospital, these well-preserved buildings are separated from the existing historic district to the north by I-40 and modern construction around the I-40/Enola Road interchange. The cottages are now owned and operated by the Western Carolina Center, a modern treatment facility for the mentally retarded that occupies an expansive, 200-acre campus primarily west of Enola Road. The center uses the former Broughton Hospital cottages for a variety of administrative offices and small activity centers for patients.

Broughton Hospital built these staff cottages in the 1920s as part of the progressive Colony Program that included smaller facilities to treat patients as well as to house the growing staff. These simple one story, frame cottages followed a limited number of side gable, hip roofed, L-plan, and gable-front designs, and were used primarily for employees of the hospital's on-site farm work program launched during the Colony Program. By the late 1920s, the cottages lined Enola Road adjacent to the fields and agricultural buildings on the south side of the hospital grounds. A row of eleven such workers' quarters stand within the existing (1987) B.H.H.D. boundaries. Interstate Highway 40 and recent commercial and office buildings now separate these cottages from the concentration of similar dwellings about 0.3 mile below the historic district, south of the interstate. It is not known how many staff cottages once lined Enola Road in this area. However, they probably formed a nearly contiguous row from near the Enola Road/Brick Street intersection southward to the south end of Western Carolina Center (Phifer 1977: 139-149; Cotton 1987: 132-133; Wylie 1986: 8: 19, 40-43; Bishir et al. 1999: 153-154).

Arranged side by side with uniform setbacks, these staff cottages are typically one story, double pile, frame dwellings with German siding, six-over-six windows, and enclosed, rear utility porches. The interiors have original four-room plans, and many retain original wood kitchen cabinetry and two-panel wood doors. There is little ornamentation and the porches have simple square posts. Narrow, paved rear driveways extend the length of the district on both sides of Enola Road. Mature trees shade many of the cottages but foundation plantings and other landscaping features are minimal. Some of the contributing cottages have replacement board-

and-batten siding but they retain original porches, windows, and other key design elements. The two non-contributing cottages have both replacement siding and replacement windows.

The recommended National Register boundary expansion for this area south of I-40 encompasses all thirty-five former Broughton Hospital staff cottages on both sides of Enola Road. Later buildings not historically associated with the hospital clearly separate this district from the surrounding parcels. The boundaries are defined by the rear driveways and property lines.

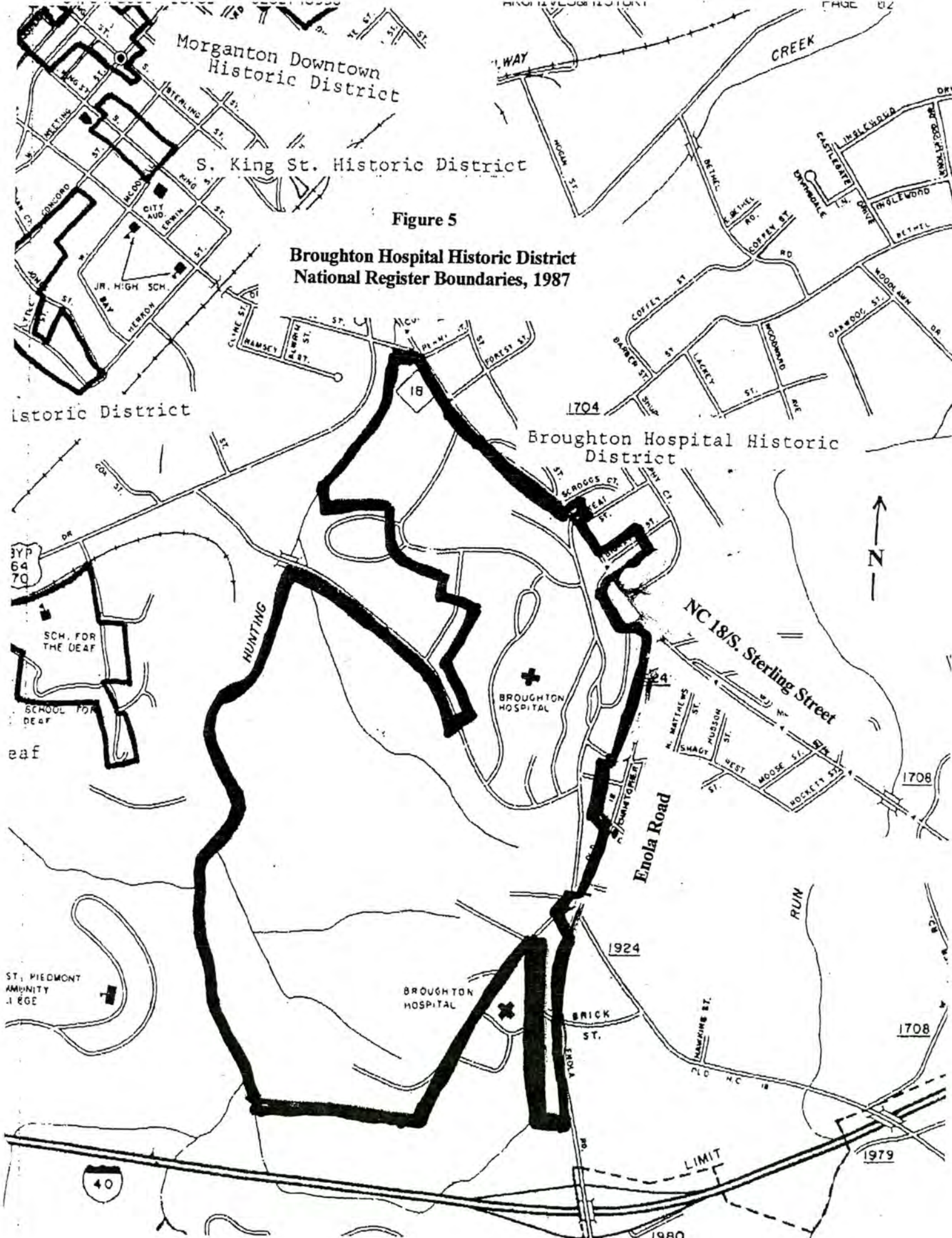


Figure 5

**Broughton Hospital Historic District
National Register Boundaries, 1987**

Morganton Downtown
Historic District

S. King St. Historic District

Historic District

Broughton Hospital Historic
District

NC 185. Sterling Street

Enola Road

BROUGHTON
HOSPITAL

BROUGHTON
HOSPITAL

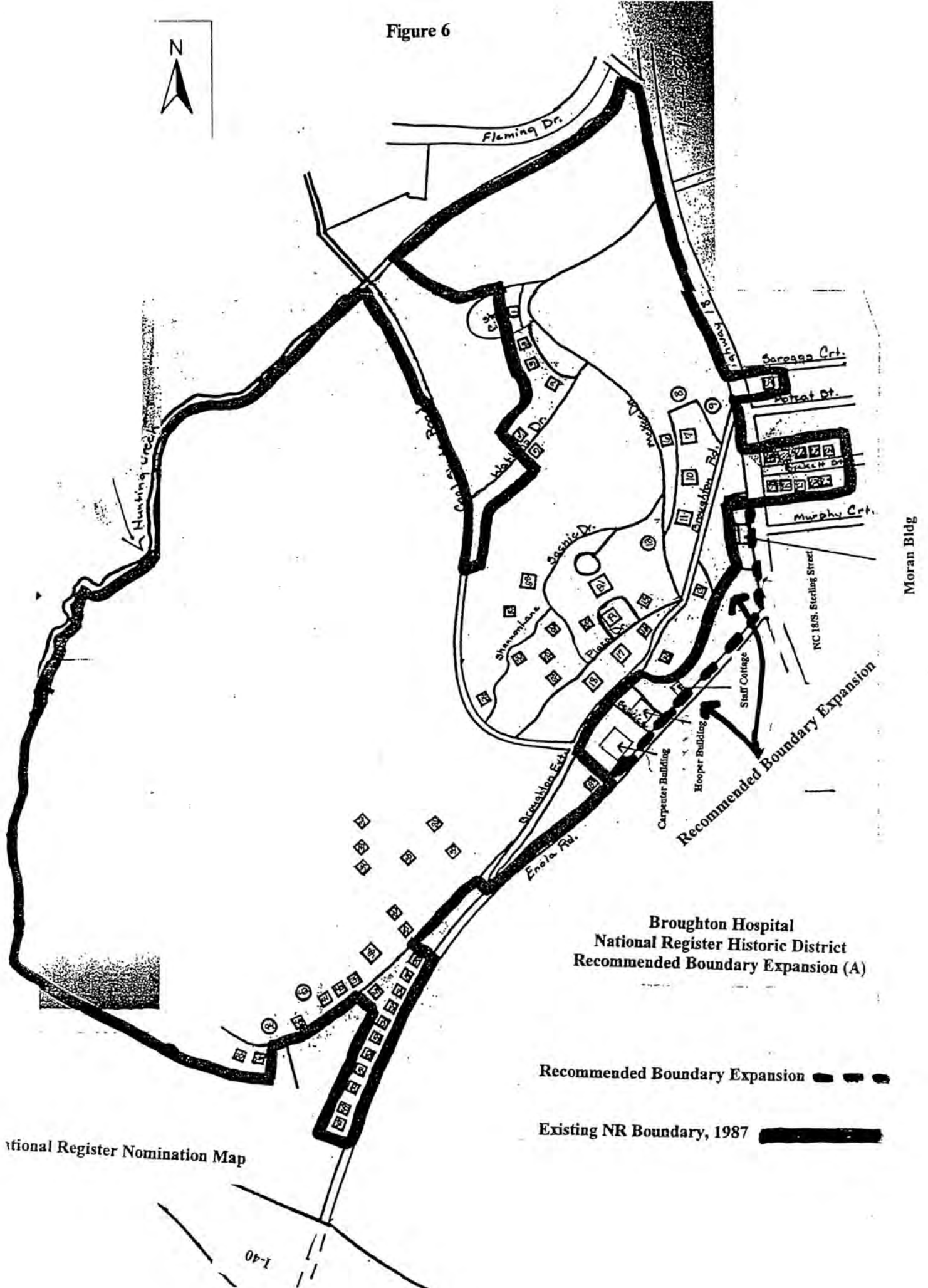
40

1708


1979




Figure 6

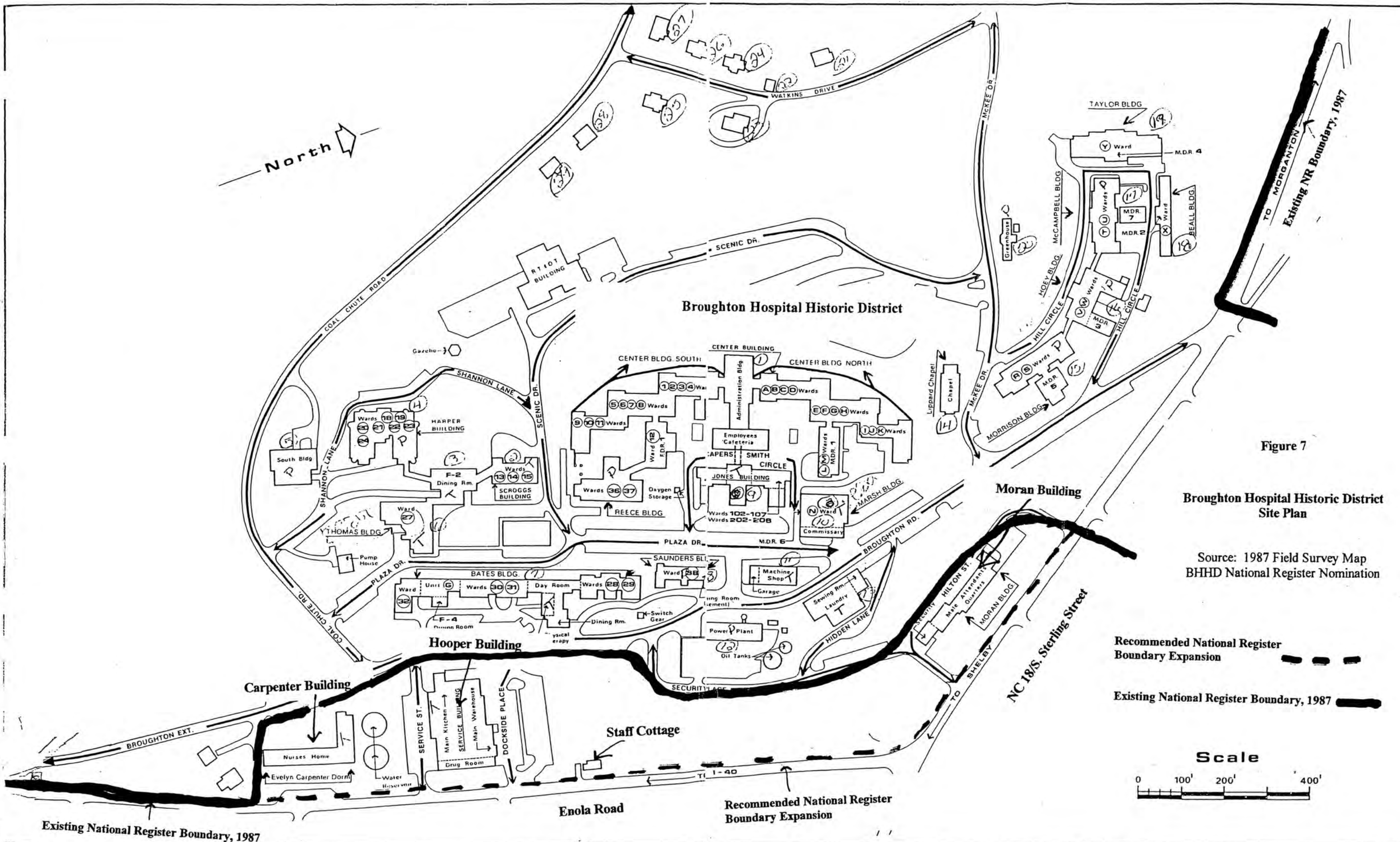


**Broughton Hospital
National Register Historic District
Recommended Boundary Expansion (A)**

Recommended Boundary Expansion 

Existing NR Boundary, 1987 

ational Register Nomination Map



Existing National Register Boundary, 1987

BROUGHTON
 Morganton,

HOSPITAL
 North Carolina

VAUGHAN / KUEHN
 LANDSCAPE ARCHITECTS
 LAND PLANNING CONSULTANTS
 908 s. kings drive, charlotte, n.c. 704-332-5884

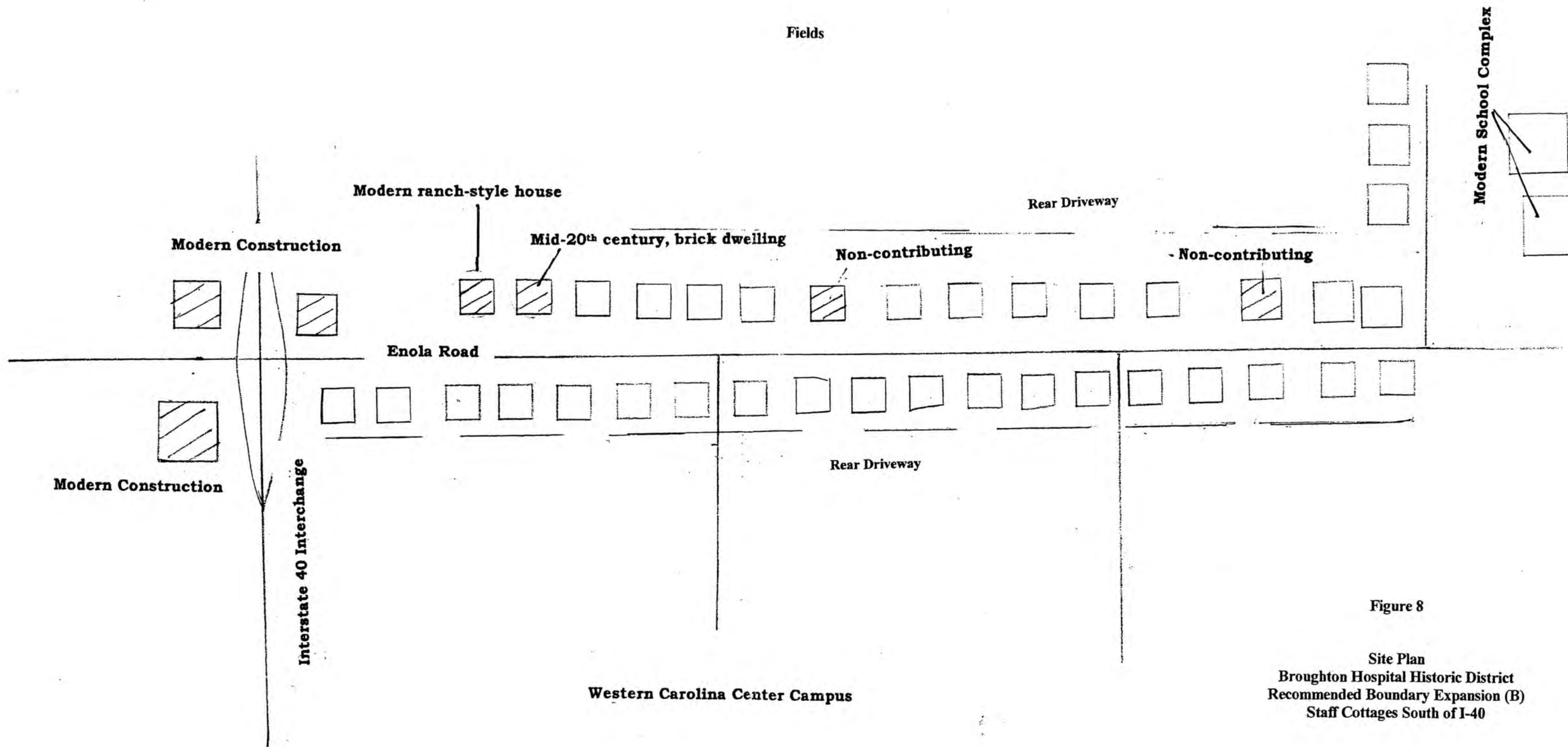
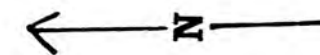


Figure 8

Site Plan
 Broughton Hospital Historic District
 Recommended Boundary Expansion (B)
 Staff Cottages South of I-40

(not to scale)



Existing Broughton Hospital Historic District (BHHD)

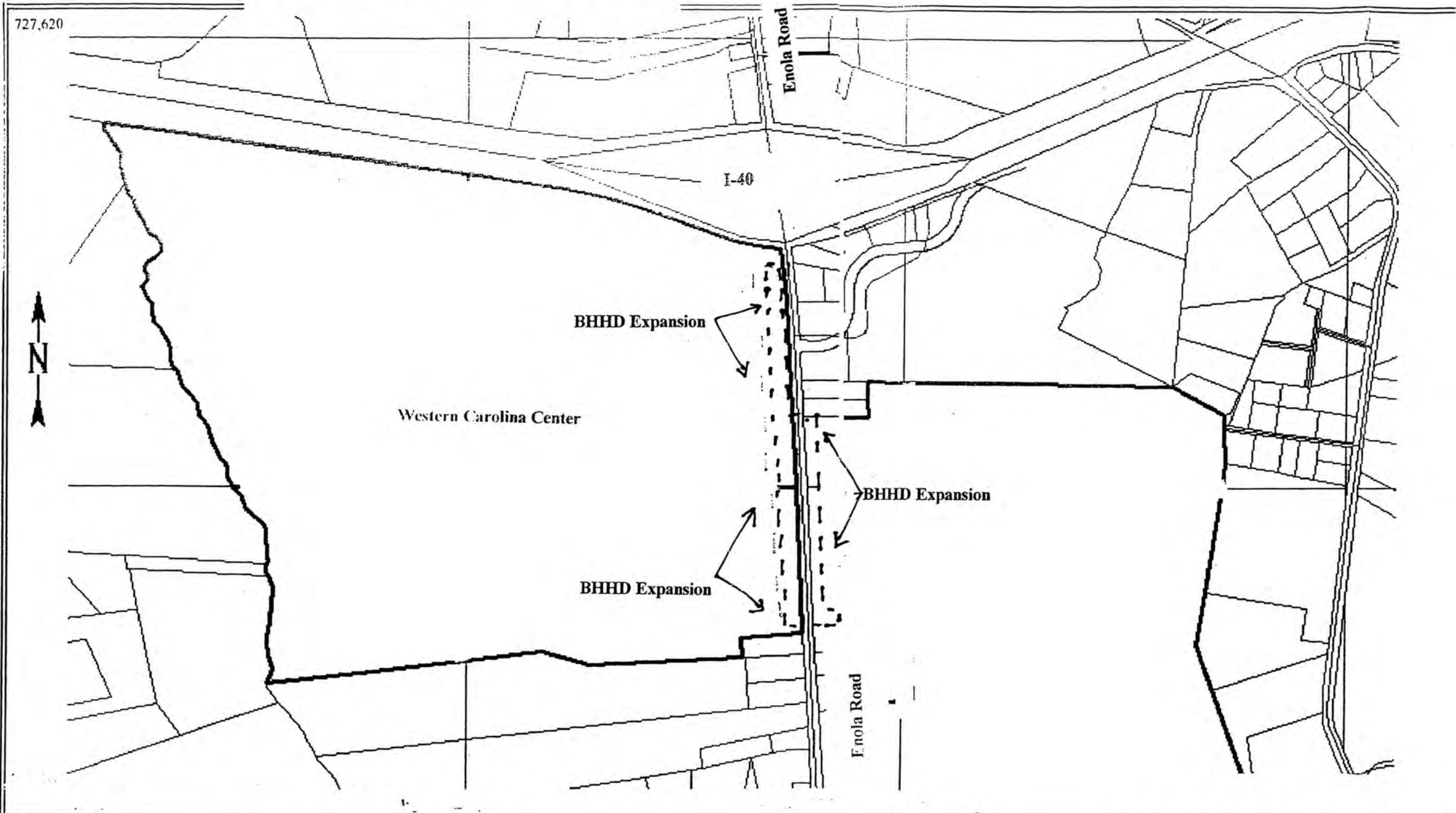


Figure 9

Broughton Hospital Historic District
Recommended Boundary Expansion (B)
Staff Cottages South of I-40

Scale 1" = Approx. 540'

Burke County Property Map, 2001



Plate 1. Broughton Hospital Historic District (B.H.H.D.), Recommended Boundary Expansion, Carpenter Building, Looking West.

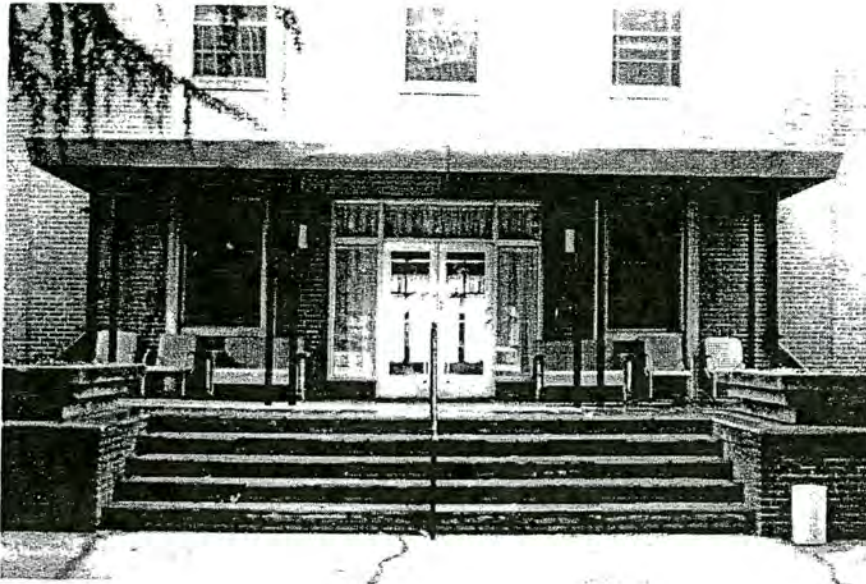


Plate 2. B.H.H.D., Recommended Boundary Expansion, Carpenter Building, Main Entry.

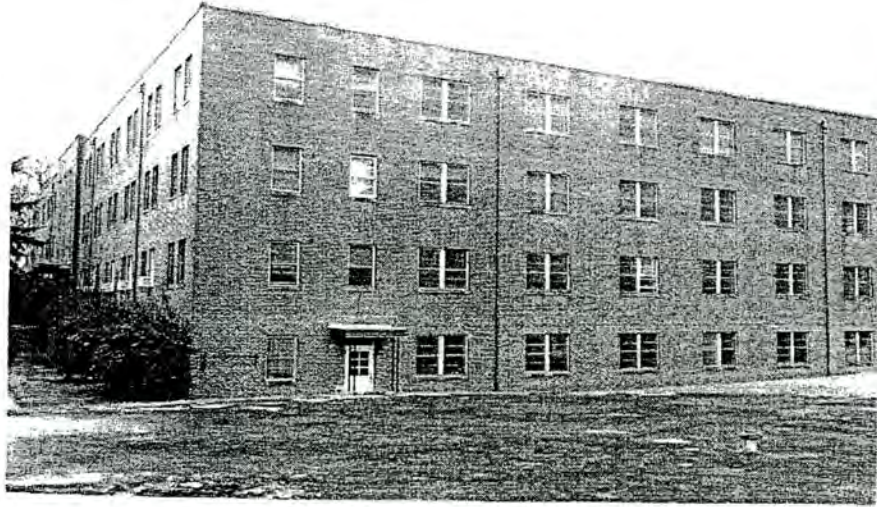


Plate 3. B.H.H.D., Recommended Boundary Expansion, Carpenter Building, North Elevation, Looking South.

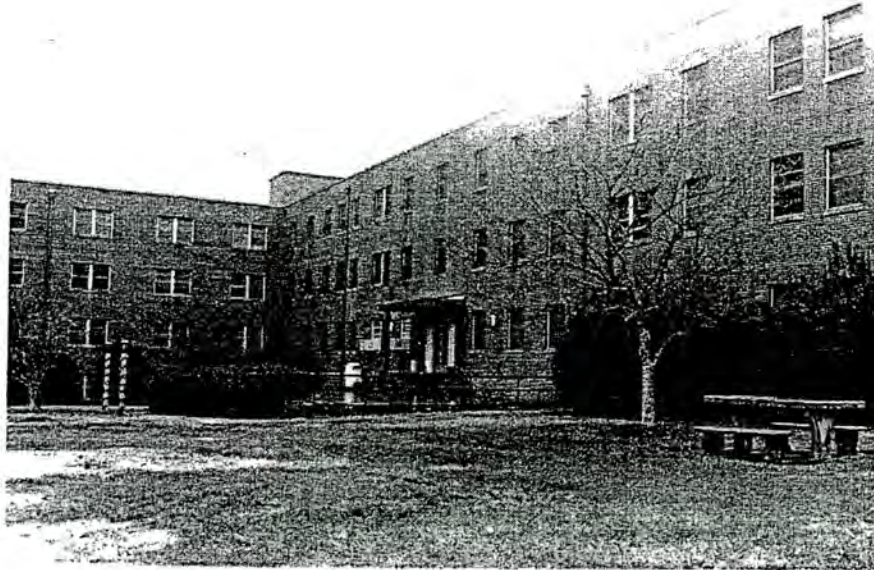


Plate 4. B.H.H.D., Recommended Boundary Expansion, Carpenter Building, Rear Elevation, Looking North.



Plate 5. B.H.H.D., Recommended Boundary Expansion, Carpenter Building, Looking South Along Enola Road Towards Dale's Market.



Plate 6. B.H.H.D., Recommended Boundary Expansion, Hooper Building, East Elevation, Looking North Along Enola Road.



Plate 7. B.H.H.D., Recommended Boundary Expansion, Hooper Building, South Elevation, Looking North.

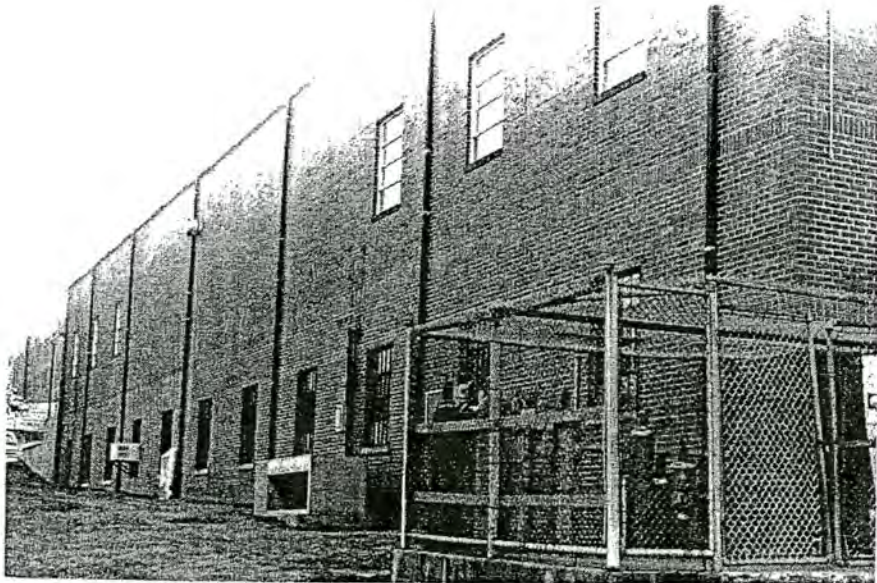


Plate 8. B.H.H.D., Recommended Boundary Expansion, Hooper Building, West Elevation, Looking Northeast.



Plate 9. B.H.H.D., Recommended Boundary Expansion, Moran Building, East Elevation, Looking North.



Plate 10. B.H.H.D., Recommended Boundary Expansion, East Elevation, Looking Northwest.



Plate 11. B.H.H.D., Recommended Boundary Expansion, Staff Cottage, Looking North.



Plate 12. B.H.H.D., Recommended Boundary Expansion, Staff Cottage, Rear Elevation, Looking South.

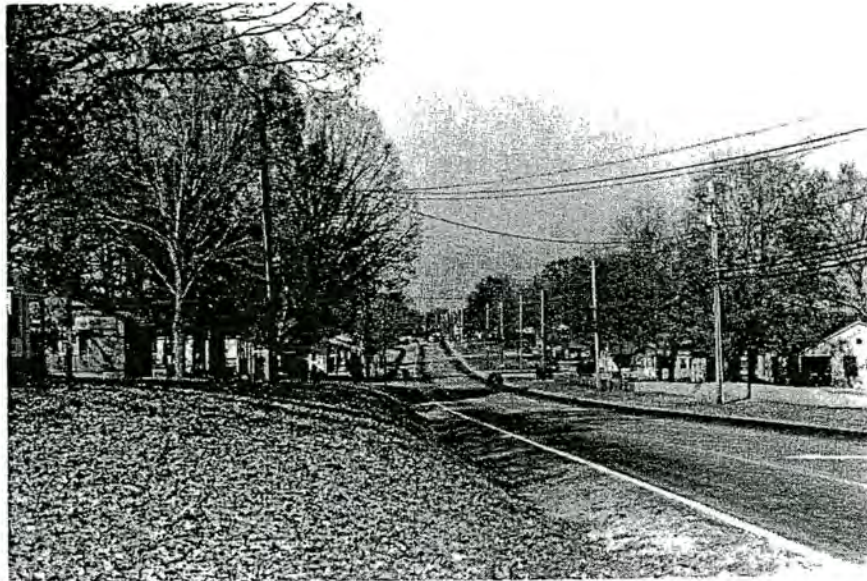


Plate 13. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Looking North Along Enola Road.



Plate 14. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Looking North Along West Side Of Enola Road.



Plate 15. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Looking North Along East Side Of Enola Road.

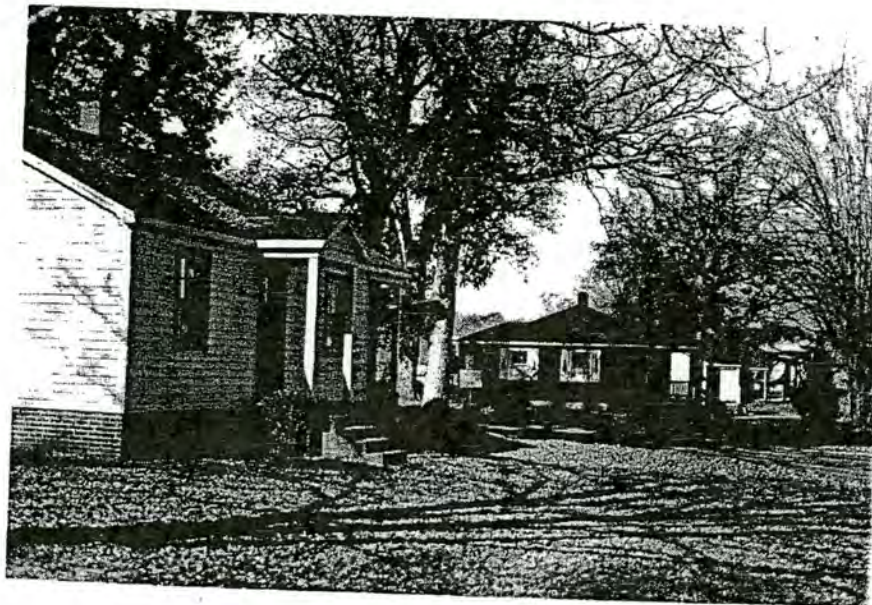


Plate 16. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Looking North Along West Side Of Enola Road.



Plate 17. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Hip-Roofed Model, Looking North Along Enola Road.



Plate 18. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Gable-Front Model, Looking North Along Enola Road.



Plate 19. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Side Gable Model, Looking North Along Enola Road.



Plate 20. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Rear Elevations, Looking North Along East Side Of Enola Road.



Plate 21. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Rear Elevations, Looking North Along West Side Of Enola Road.

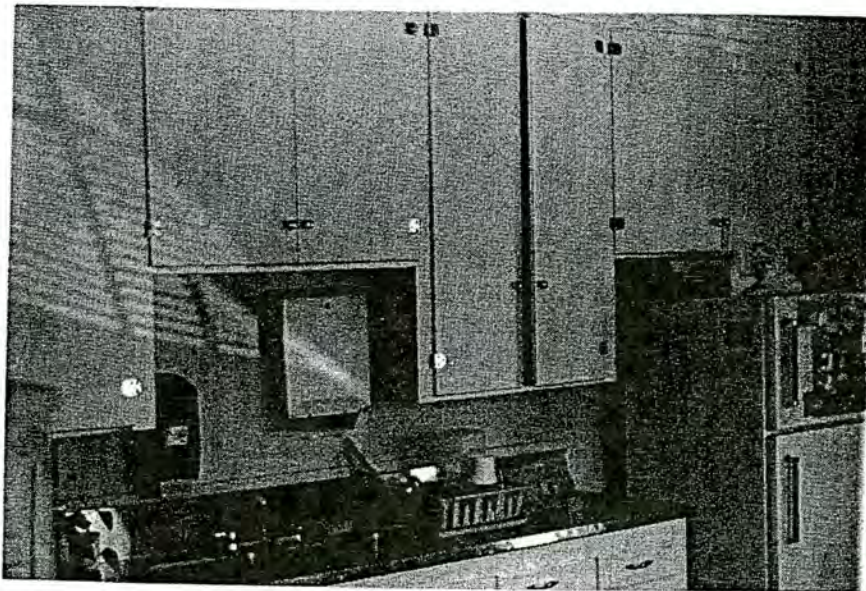


Plate 22. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Kitchen Cabinets.

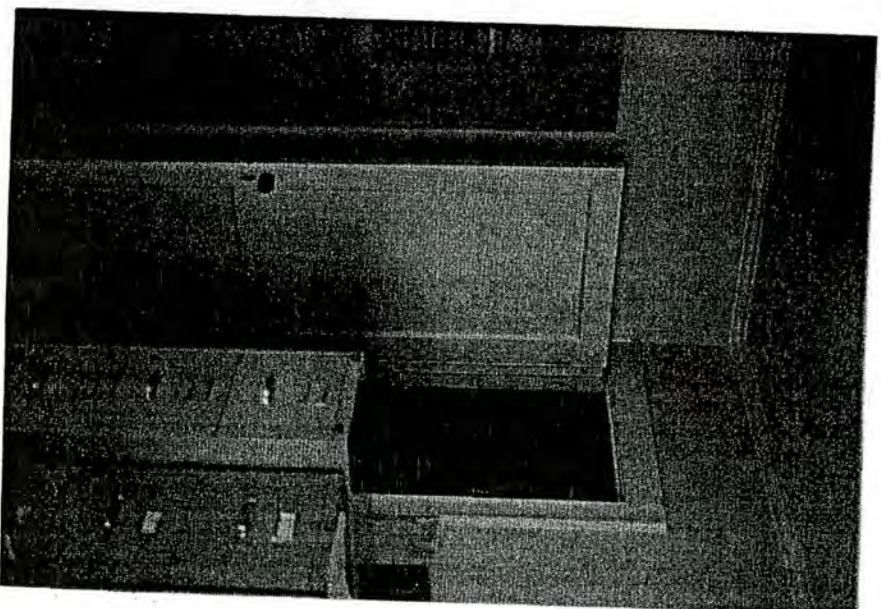


Plate 23. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40,
Interior View.

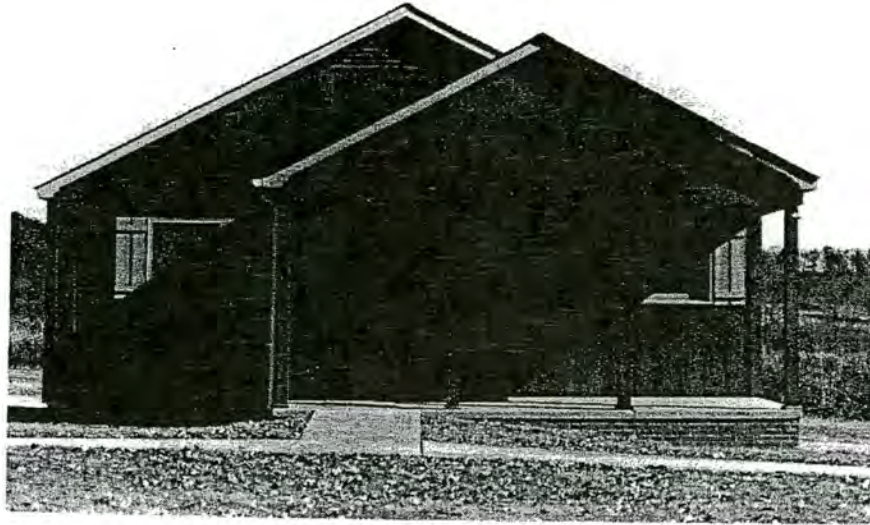


Plate 24. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Non-Contributing Cottage, East Side Enola Road.



Plate 25. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Mid-Twentieth Century, Brick Dwelling Located Outside District, Looking East.



Plate 26. B.H.H.D., Recommended Boundary Expansion, Staff Cottages South of I-40, Western Carolina Center Campus, Looking West From Enola Road.

No 2. U.S.B. Dale's Market (N.R. 1986)
Southeast corner of Enola Road and Dale Circle
Morganton, Burke County

A well-preserved, rural grocery store, Dale's Market was built ca. 1900 as a replacement for an earlier store that stood across Dale Circle where a modern laundry is now situated. A one story, narrow, brick commercial building, Dale's Market has decorative brickwork above the retail level, and large display windows are framed by fluted, square, cast-iron pilasters that flank the slightly recessed entrance. The simple interior retains its original plaster walls, hardwood floors, and open plan. It is notable that an original hand pump still stands outside next to the entrance, and gasoline pumps had been added in the 1920s, but have since been removed (**Figure 10**). Still in use, the market was nominated to the National Register as an architecturally significant and rare example of a rural community store. Although now located within Morganton, the store was built on the outskirts of Morganton in the Enola Road community across from the Broughton Hospital campus. Virtually unchanged since its National Register listing in 1986, Dale's Market remains eligible under Criterion C for architecture (Wylie 1986). No National Register boundary map is available for this property although the nomination states, "The nominated property includes the store and the lot on which it is situated. The boundaries are formed by Dale Circle to the north, Enola Road to the west, and residential property to the east and south" (Wylie 1986).

Proposed Changes to the U.S.B. Dale's Market National Register Boundaries (Figures 11-12
Plates 27-33)

Based upon the Phase II field work, it is recommended that the Dale's Market National Register nomination be amended to include the owner's dwelling, an imposing, ca. 1925 bungalow that stands on the tree-shaded hill behind the store. Dale's Market is already listed in the National Register under Criterion C for architecture, and it is recommended that the Dale house also be added to the nomination under Criterion C for architecture as a fine example of a Craftsman style bungalow dating to the 1920s. The Dale house is a substantial and illustrative, frame bungalow with a low-pitched, side gable roof, broad eaves, Craftsman style knee brackets, and a wide, engaged porch supported by concrete piers on brick pedestals. The house has shingled gables, weatherboard siding, three-over-one windows, and a shed-roofed bay on the north elevation. Standing in the side yard of the house is a well that is covered by a pyramidal roof supported by wooden piers on fieldstone pedestals. Vacant since the 1960s, the house retains its exterior integrity although the structural condition of the interior is unknown.

The market and house complex are also recommended as eligible under Criterion A for commerce as a good, but now rare, illustration of a traditional commercial pattern in rural communities (see Historic Context, pp. 16). The geographical association of owner's house to country store was common before the age of the automobile when merchants typically lived either above their stores or in nearby dwellings. Because transportation for farmers and other rural residents was difficult and limited prior to the introduction of the automobile and highway improvements, crossroads general stores were scattered throughout the countryside to serve the varied needs of local farming families, and merchants, who often farmed or engaged in other endeavors, had to live near their stores. However, since World War I, the explosive growth of automotive traffic has significantly altered traditional patterns of commerce in farming communities. The car freed both merchant and customer from their rural isolation, and as a consequence, commerce tended to become centralized in larger towns or cities where a wider variety of goods and services were available, often for lower prices. Thus, throughout the twentieth century, rural stores have gradually become obsolete, and such mercantile operations, either individually or as part of a rural store and house complex, have not survived in great numbers. Although the Enola Road Community has been subsumed within Morganton, Dale's

Market, and the Dale house to the rear, retain their rural feeling. The store fronts directly on the road, while the house occupies a spacious, tree-shaded hill behind the store.

The National Register boundaries include the parcel that conforms to the boundaries defined in the National Register nomination for Dale's Market as well as the surrounding parcel on which the Dale house sits.

Figure 10

U.S.B. Dale's Market
Sanborn Map, 1961

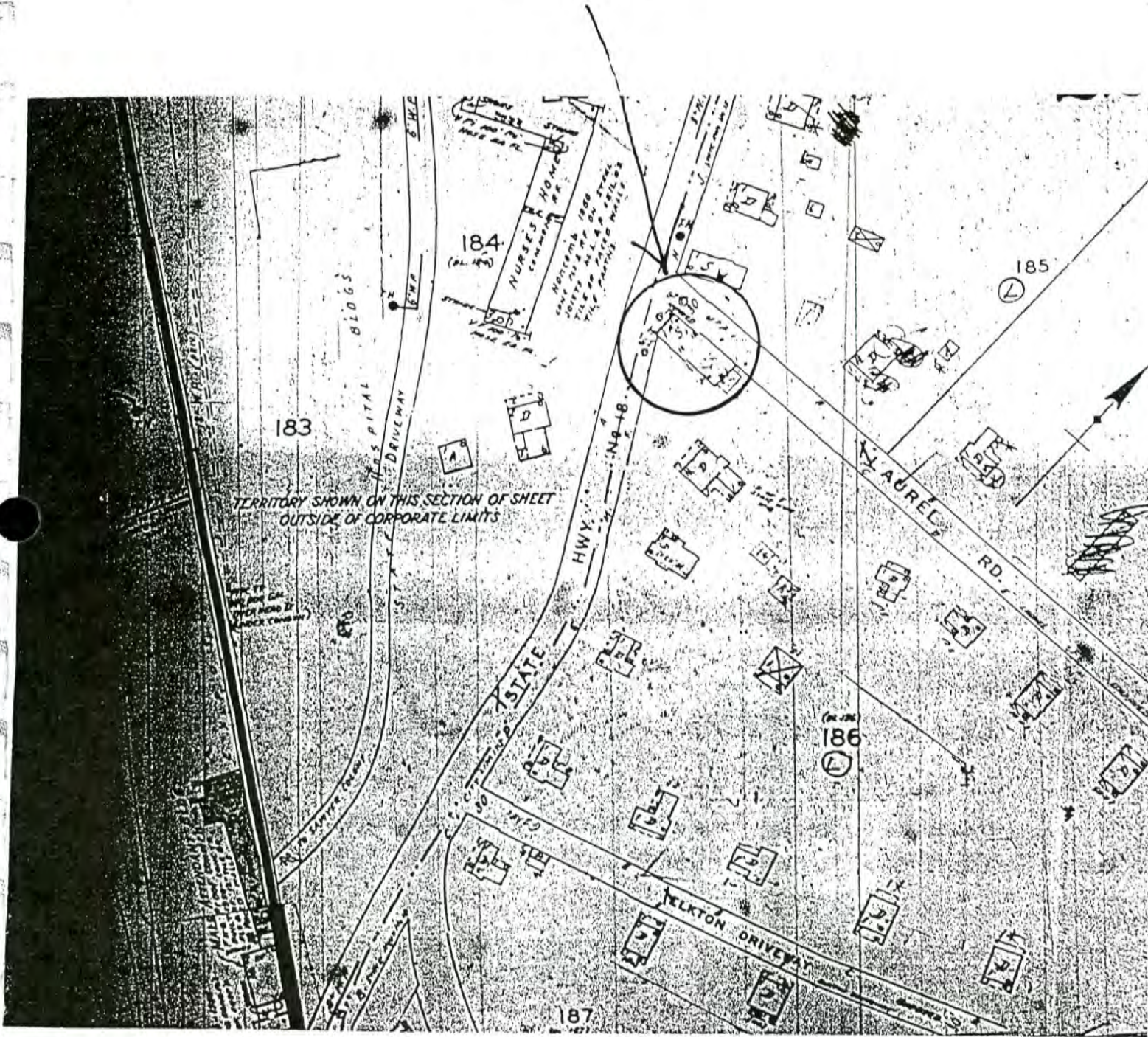
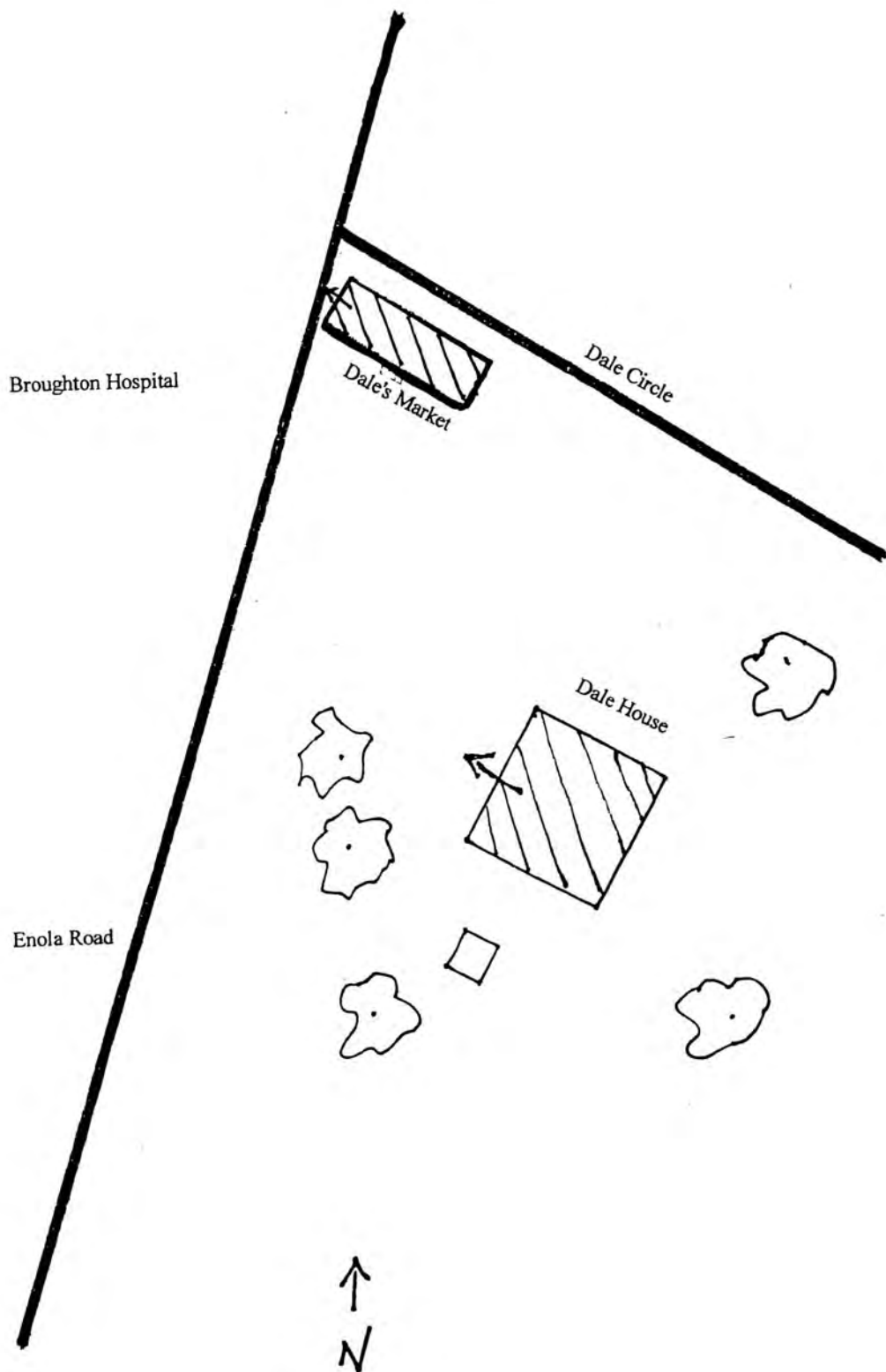


Figure 11

U.S.B. Dale's Market
Site Plan



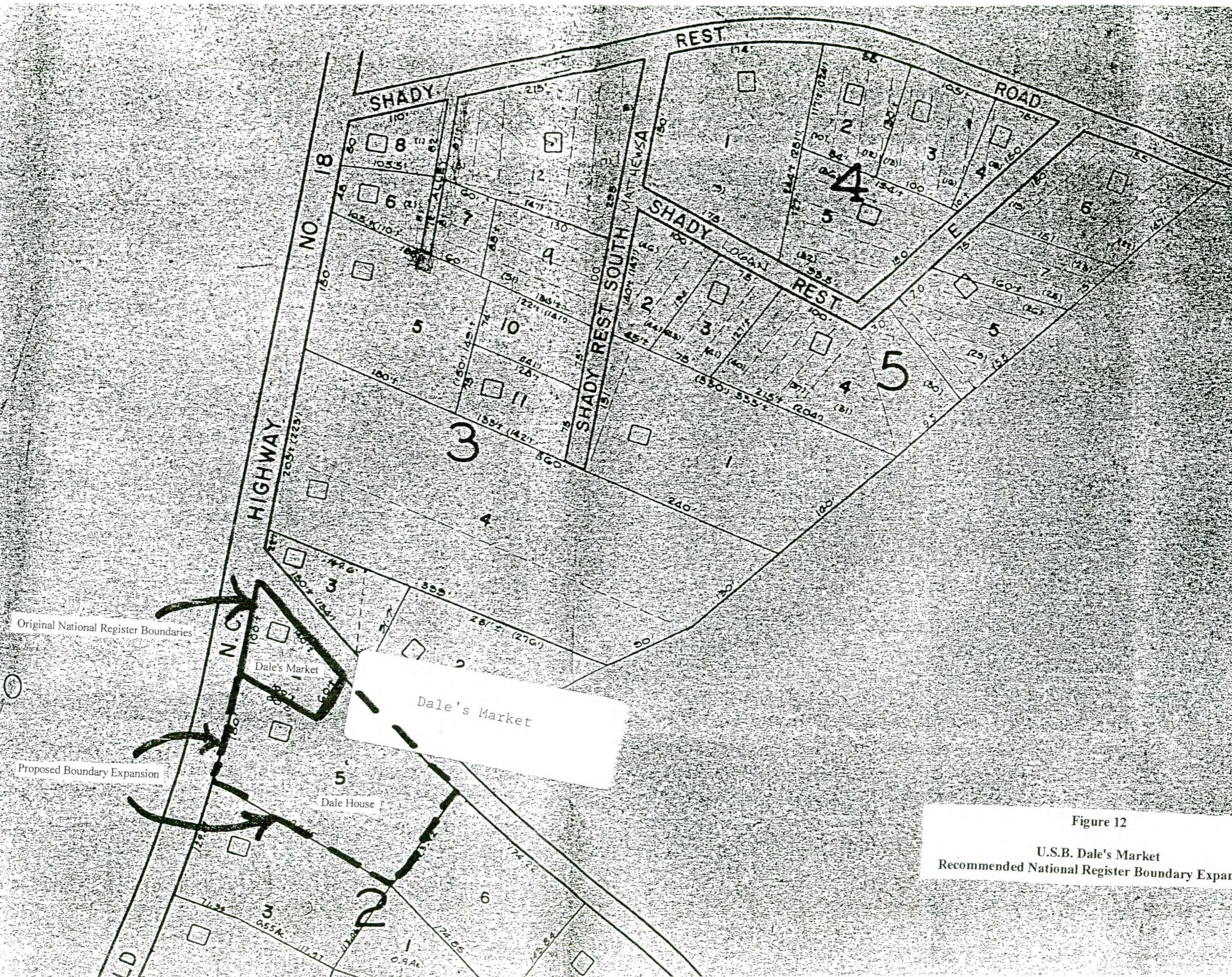
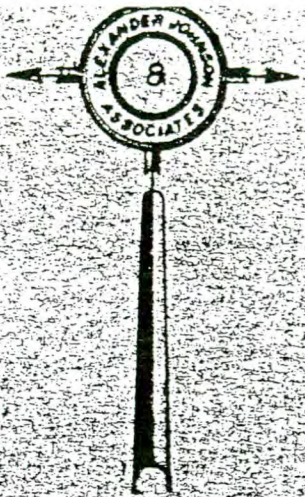


Figure 12
U.S.B. Dale's Market
Recommended National Register Boundary Expansion

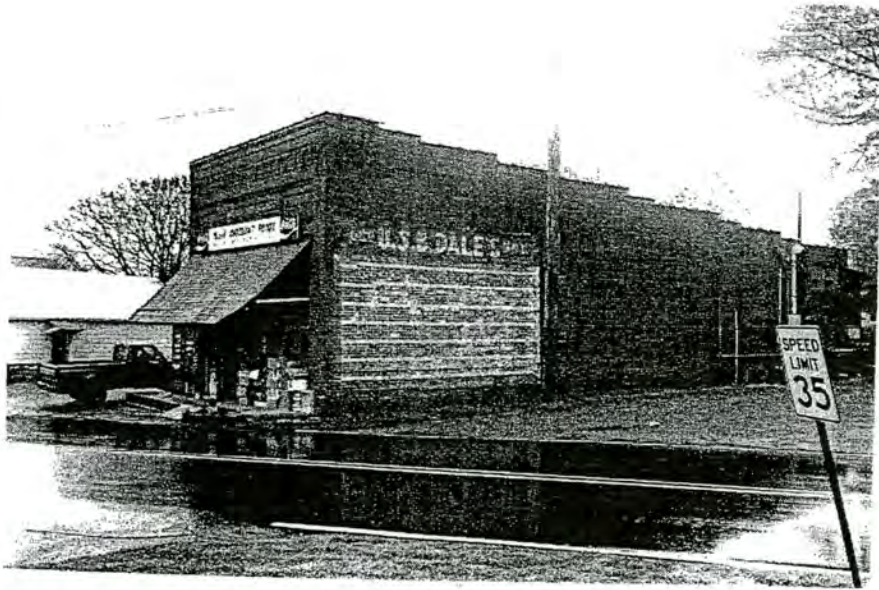


Plate 27. U.S.B. Dale's Market, Facade and South Elevation, Looking Northeast.



Plate 28. U.S.B. Dale's Market, Store Facade and Dale House, Looking Southeast.



Plate 29. U.S.B. Dale's Market, Dale House, Facade and North Elevation, Looking Southeast.



Plate 30. U.S.B. Dale's Market, Dale House, Facade, Looking Southeast.

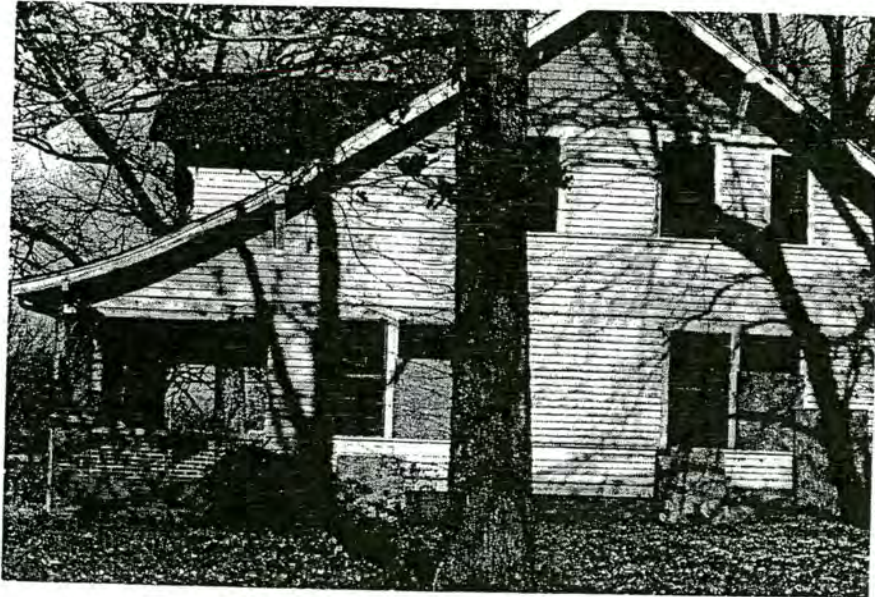


Plate 31. U.S.B. Dale's Market, Dale House, South Elevation, Looking North



Plate 32. U.S.B. Dale's Market, Dale House, Rear Ell, Looking Northwest.



Plate 33. U.S.B. Dale's Market, Dale House, South Elevation and Well, Looking Northwest.

Properties Evaluated Intensively and Recommended as Eligible for the National Register

No. 3 Mull's Store and House

East and west sides of Enola Road, at junctions with Brick Street and Vocational Rehabilitation Road, Morganton, Burke County

Date of Construction

Store - 1928; House - 1937

Outbuildings

Frame Garage

Setting and Landscape Description

Mull's Store occupies a parcel on the west side of two-lane Enola Road that separates the Broughton Hospital campus from the road. The long, narrow building stretches along the highway with parking in front. Across Enola Road, the 1937 Mull House is situated on a large, down sloping lot surrounded by houses of both the prewar and postwar eras. The tree-shaded lot includes mature plantings, and the driveway and garage are reached from Brick Street.

Physical Description and Evaluation of Integrity (Figure 13) (Plates 34-43)

The Mull Store is a well-preserved, one story, cast stone building with a flat roof, original double leaf doors with horizontal panels and upper lights, and storefront display windows. Two concrete block additions (ca. 1950) have been made to the store. On the south end, a two story section houses feed and fertilizer storage on the ground level and an office in the upper story. At the north end, a one story section houses a meat locker and restrooms. The interior is intact with hardwood floors, stuccoed walls, and a composition tile ceiling. The open interior is supported by box piers and is divided into aisles with a meat department at the north end, and a door leading into the south end storage room. The store has not been altered since its ca. 1950 additions and retains its architectural integrity.

The Mull House is a stone cottage with an L-plan, a cross gable roof, and a bracketed off-center entrance. A stone chimney is situated at the junction of the two main blocks of the house. The house has a stone deck, but no porch, and paired six-over-six windows. Because of the slope of the land, there is a half-basement along the rear elevation. A wooden staircase has been added to the rear entrance in recent years. The interior was inaccessible, but according to the Mulls' daughter is unchanged. The property includes a front gable, asbestos shingled garage. The Mull house retains its architectural integrity.

Historical Background

As with the nearby Dale house and store, Paul Mull, a descendant of one of Burke County's eighteenth century mercantile families, also operated a store along State Highway 18 (Enola Road), and he and his wife built their house across the road from the store several years later. The Mulls are reputed to have lived in the store until their stone cottage across Enola Road could be built in 1937. The store had been erected in 1928 following Paul Mull's 1927 marriage to Eula Christenbury, and provided groceries, feed, fertilizer, and other general supplies to nearby farm families. The Mulls operated the store until recent years, and Mrs. Mull still lives in the family house (Yount interview 2001).

Evaluation of Eligibility (Figure 14)

The Mull Store and House is recommended for the National Register under Criterion A for commerce as a good, but now rare, illustration of the traditional association of owner's house to

general store (see Historic Context, pp. 16). Although it was common before the age of the automobile for merchants to live either above their stores or in nearby dwellings, this relationship of store and owner's house is now rare. Prior to the introduction of the automobile and subsequent highway improvements, transportation for farmers and other rural residents was difficult and limited. Crossroads stores were dispersed throughout the countryside to serve the varied needs of local farming families, and merchants, who often farmed or engaged in other endeavors, had to live near their stores. Since World War I, the explosive growth of automotive traffic has significantly altered these traditional patterns of commerce in farming communities. The car freed both rural customers and merchants from this isolation, and commerce tended to become centralized in larger towns or cities where a variety of goods and services were available, often at lower prices. Throughout the past century, rural stores have become increasingly obsolete, and such mercantile operations, either individually or as part of a rural store and house complex, have not survived in great numbers. Although the Enola Road Community has been subsumed within Morganton, the Mull Store and House, sited along winding, two-lane Enola Road, retain much of their rural feeling. The store stretches along the highway, while the house is situated on a large, tree-shaded lot opposite the store.

The house and store complex are also recommended under Criterion C for architecture as a good example of early twentieth century, roadside architecture. The store was constructed of cast stone, a concrete product fabricated to look like rock-faced stone, which became popular by the 1920s as other forms of concrete construction came into vogue. The material was useful for a variety of projects because cast stone was largely fireproof and easy to assemble. With little ornamentation, the largely unchanged store illustrates simple, rural stores of the early twentieth century. Across Enola Road, the Mull house was built as a restrained version of the English cottage style that was popular in Burke County. The style, with its asymmetrical massing and picturesque elements, was particularly suited to the stone construction so typical of western North Carolina architecture during the early twentieth century. Many such designs were built in Burke County during the period, and the Mull house is fine example of the style.

The Mull Store and House is not eligible under Criterion B because the property is not associated with individuals whose activities were demonstrably important within a local, state, or national historic context. Finally, the property is not considered eligible under Criterion D because the architectural component is not likely to yield information important in the history of building technology.

The proposed National Register boundaries are the tax parcels on which the house and store sit. The two parcels together encompass the house, store, and garage, all of which are contributing resources.

Figure 13

Mull Store and House
Site Plan

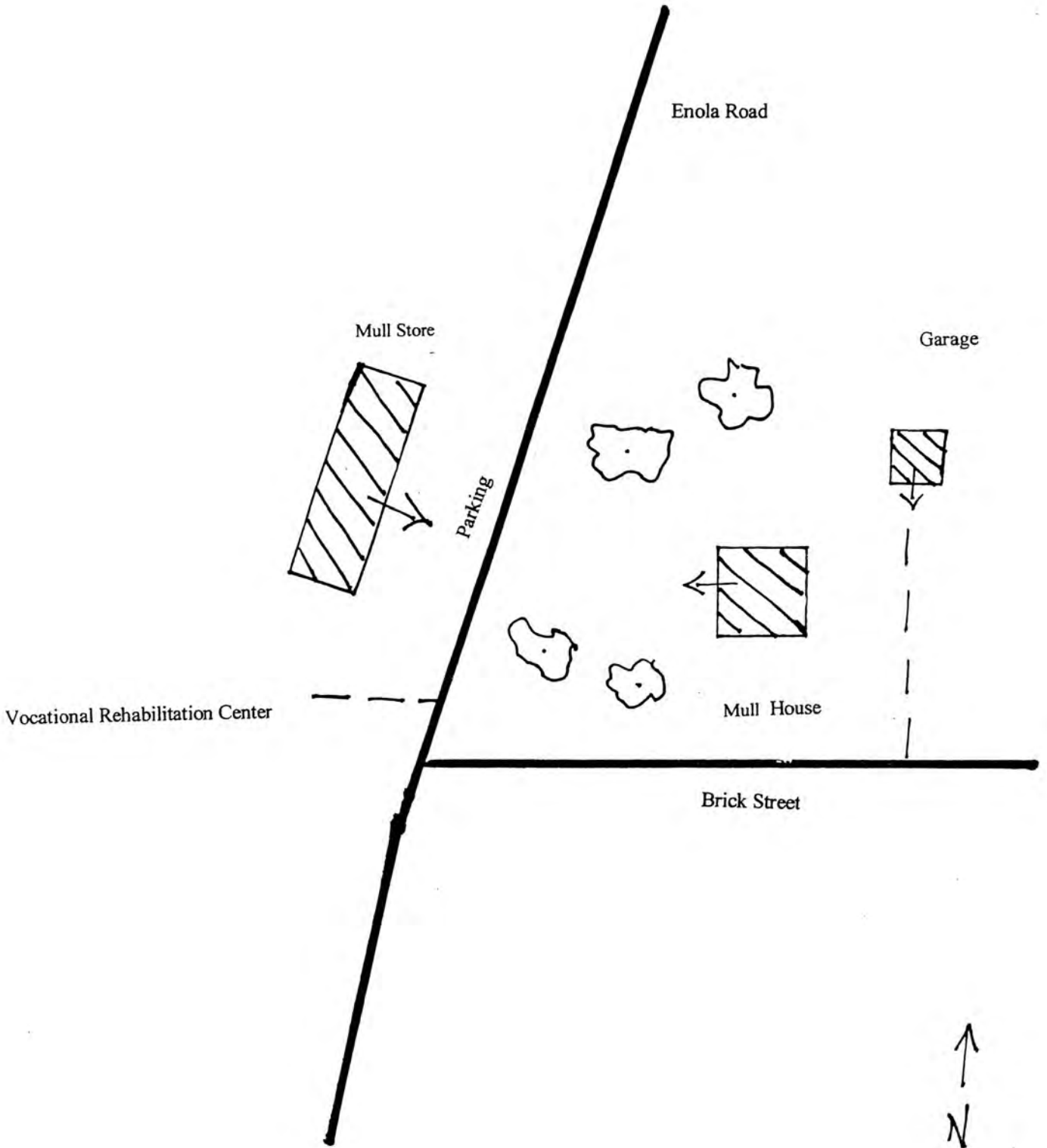


Figure 14

Mull Store and House
Recommended National Register Boundaries

Source:
Burke County Tax Map

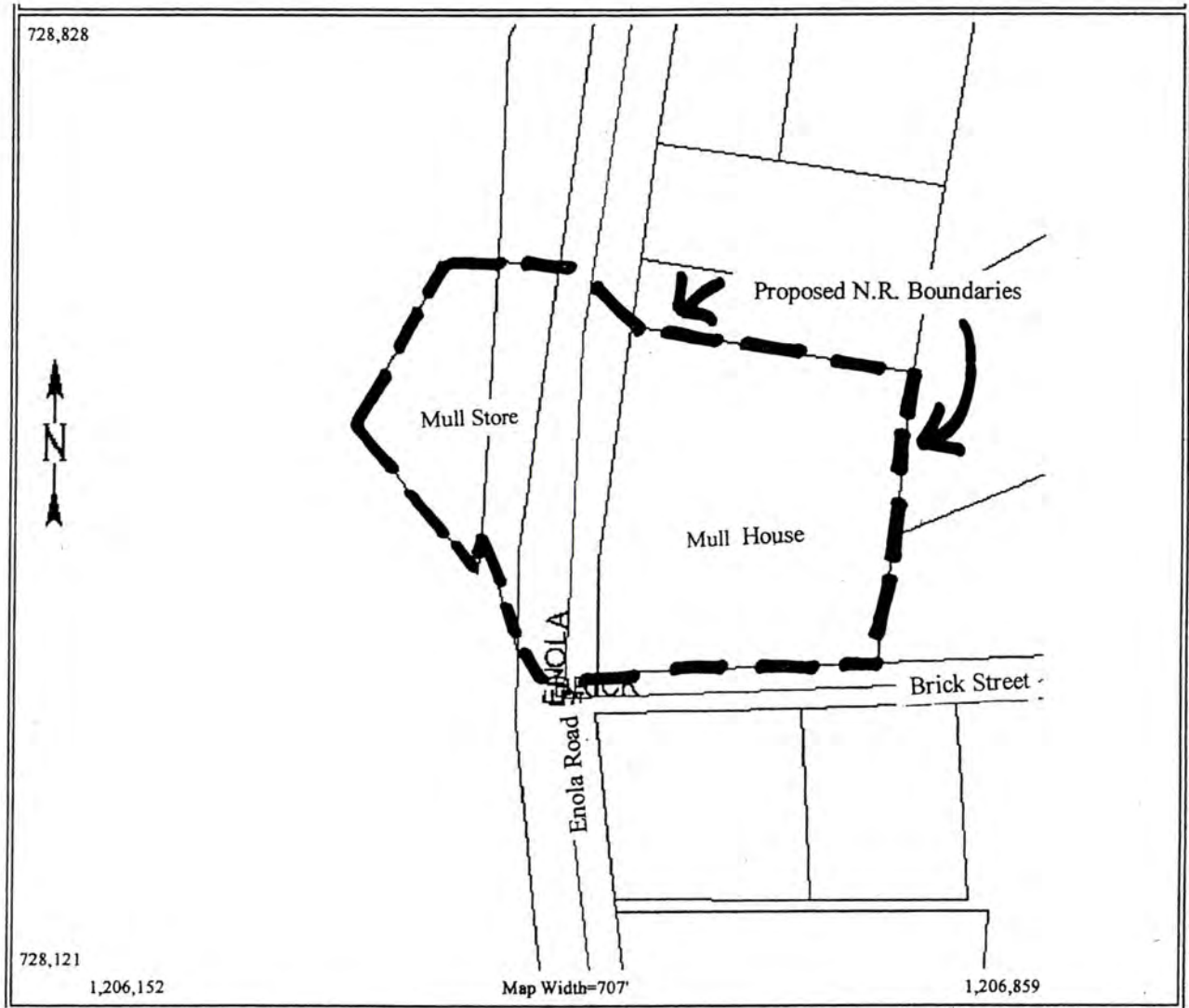




Plate 34. Mull Store and House, Facade of Store, Looking Southwest.

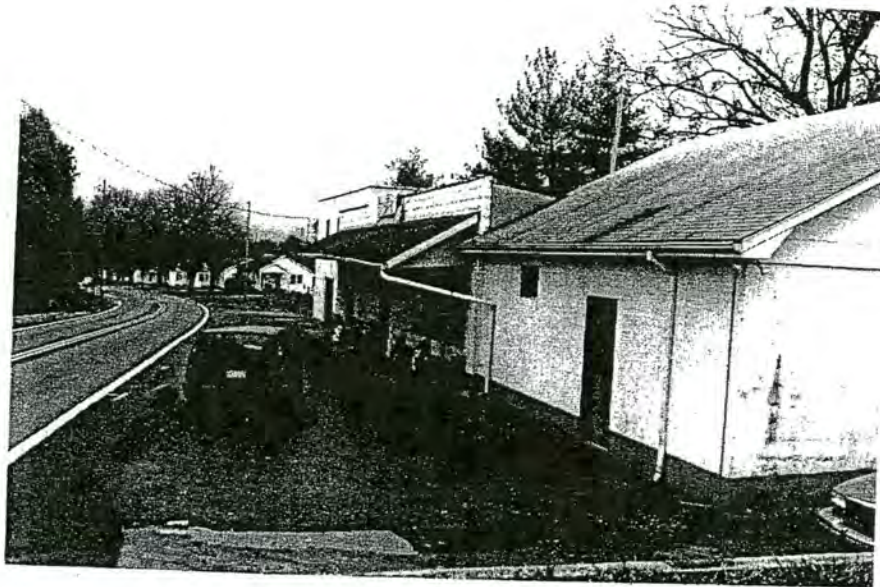


Plate 35. Mull's Store and House, Looking South From Store Parking Lot Along Enola Road.

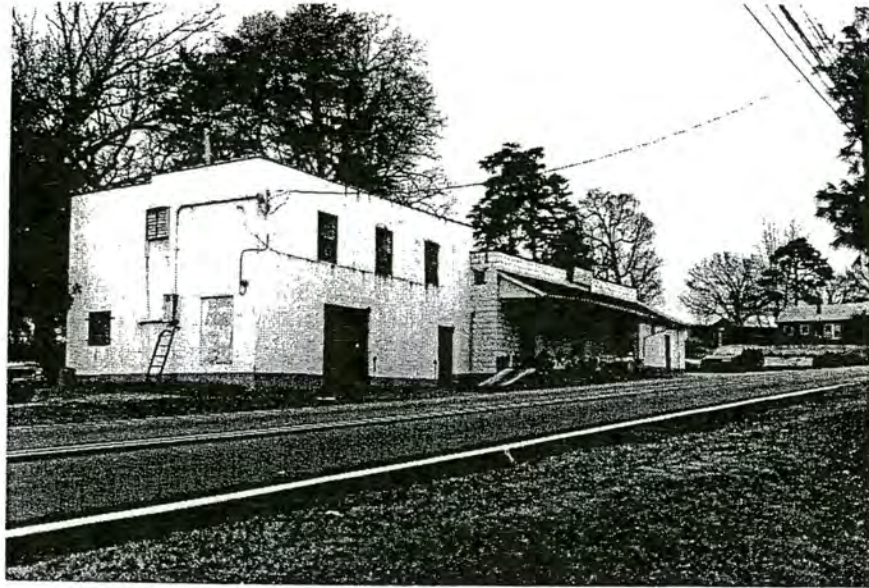


Plate 36. Mull Store and House, Facade and South Elevation of Store, Looking Northwest.



Plate 37. Mull's Store and House, Entrance Detail of Store.



Plate 38. Mull Store and House, Interior of Original Store, Looking North.



Plate 39. Mull's Store and House, Store Interior, Cold Storage Room Along North Wall.



Plate 40. Mull Store and House, Mull House, Facade and South Elevation Looking East.



Plate 41. Mull's Store and House, House Facade and North Elevation, Looking Southeast.



Plate 42. Mull Store and House, Rear Elevation of House, Looking Northwest.

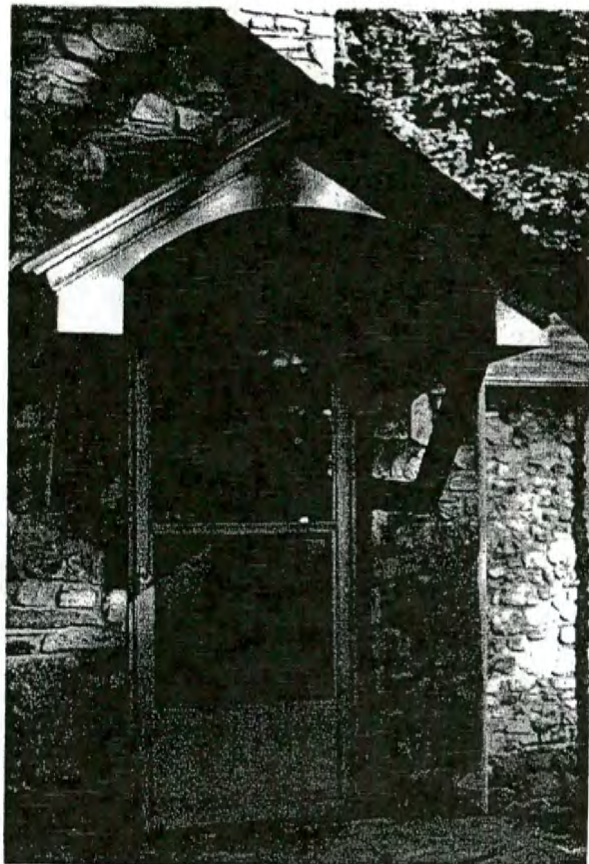


Plate 43. Mull's Store and House, Detail of House Entrance.

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- . "National Register Nomination for the North Carolina School for the Deaf Historic District." North Carolina Division of Archives and History, Raleigh, North Carolina. 1986.
- . "National Register Nomination for the Broughton Hospital Historic District." North Carolina Division of Archives and History, Raleigh, North Carolina. 1986.
- . "National Register Nomination for U.S.B. Dale's Market." North Carolina Division of Archives and History, Raleigh, North Carolina. 1986.

Yount, Kay Mull. Interview with principal investigators. 12 December 2001. Morganton,
North Carolina.

APPENDIX A:

CONCURRENCE FORM

EVALUATIONS AND PHOTOGRAPHIC INVENTORY

FIELD SURVEY MAP



M. Penney

North Carolina Department of Cultural Resources
State Historic Preservation Office
David L. S. Brook, Administrator

Michael F. Easley, Governor
Lisbeth C. Evans, Secretary
Jeffrey J. Crow, Deputy Secretary
Office of Archives and History

Division of Historical Resources
David L. S. Brook, Director



June 26, 2002

MEMORANDUM

TO: William D. Gilmore, Manager
Project Development and Environmental Analysis Branch
Department of Transportation, Division of Highways

FROM: David Brook *for David Brook*

SUBJECT: Draft Historical Architectural Survey Report, Enola Road from I-40 to NC 18,
Morganton, U-2551, Burke County, ER 02-9941

Thank you for your letter of May 21, 2002, transmitting the draft survey report by Mattson, Alexander and Associates, Inc. concerning the above project. We regret that due to staff vacancies we were unable to respond in a timelier manner.

For purposes of compliance with Section 106 of the National Historic Preservation Act, we concur that the following properties are eligible for listing in the National Register of Historic Places:

- Broughton Hospital Historic District National Register amendment, including the Carpenter Building, Hooper Building, Moran Building, and the Staff Cottages south of I-40 under Criteria A, B and C. The district and boundary extension is significant in the areas of health care and architecture as well as for its association with Dr. Patrick Livingston Murphy.
- Dale's Market National Register amendment to include the Dale House under Criteria A and C as a rare illustration of a traditional commercial pattern in rural communities and as a fine example of a Craftsman style house.
- Mull's Store and House under Criteria A and C as a good, but now rare, illustration of the traditional association of the owner's house to general store and a good example of roadside architecture.

The following properties do not appear to be eligible for the National Register or to warrant further study:

Houses 4-23

We have kept one copy of the draft report as a reference and clearly marked its draft status. We will return this copy upon receipt of the final report.

	Location	Mailing Address	Telephone/Fax
Administration	507 N. Blount St, Raleigh, NC	4617 Mail Service Center, Raleigh 27699-4617	(919) 733-4763 • 733-8653
Restoration	515 N. Blount St, Raleigh, NC	4613 Mail Service Center, Raleigh 27699-4613	(919) 733-6547 • 715-4801
Survey & Planning	515 N. Blount St, Raleigh, NC	4618 Mail Service Center, Raleigh 27699-4618	(919) 733-4763 • 715-4801

Page 2
William D. Gilmore
June 26, 2002

The above comments are made pursuant to Section 106 of National Historic Preservation Act and Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, please contact Renee Gledhill-Earley, environmental review coordinator, at 919/733-4763. In all future communication concerning this project, please cite the above referenced tracking number.

cc: Barbara Church, NCDOT
Matteson, Alexander & Associates

**PROPERTIES NOT RECOMMENDED FOR FURTHER STUDY
(Keyed to Survey Map)**

No. 4 House

N.C. 18 at junction with Enola Road, Morganton

The owners of this hip roofed bungalow report that it has been moved to the site. The house has an engaged porch, weatherboard siding, and four-over-four windows. The property no longer retains its integrity of setting and also lacks any historical or architectural significance.

No. 5 House

Shady Rest Road at intersection with Christopher Street, Morganton

The hip roofed dwelling has an engaged porch, but replacement one-over-one windows, vinyl siding, and an altered rear ell with an added sliding glass door. The house no longer retains sufficient integrity to meet National Register criteria, and is not located within a historic district. The surrounding neighborhood contains 1950s ranch houses, altered pre-1952 dwellings, and modern residences.

No. 6 House

Dale Circle, Morganton

This altered, side gable bungalow was built by a member of the Dale family. The house has a rebuilt porch, replacement windows, and is now in deteriorated condition. The house lacks sufficient integrity to merit National Register eligibility.

No. 7 House

East side of Enola Road at intersection with Shady Rest Road, Morganton

This hip roofed house has an engaged porch, a hip roofed dormer, replacement two-over-two, horizontal sash windows, and asbestos siding. Not built as staff housing by Broughton Hospital, the house lacks any historical or architectural significance and no longer retains sufficient integrity to meet National Register criteria.

No. 8 House

East side of Enola Road, south of intersection with Shady Rest Road, Morganton

This hip roofed dwelling has an engaged porch, a hip roofed dormer, replacement two-over-two, horizontal sash windows, and aluminum siding. Not built as staff housing by Broughton Hospital, the house lacks any historical or architectural significance and no longer retains sufficient integrity to meet National Register criteria.

No. 9 House

East side of Enola Road, south of intersection with Shady Rest Road, Morganton

This altered, frame bungalow has a front gable dormer, a front addition, and German siding. The house has replacement two-over-two, horizontal sash windows and replacement porch posts. Not built as staff housing by Broughton Hospital, the house lacks any historical or architectural significance and no longer retains sufficient integrity to meet National Register criteria.

No. 10 House

East side of Enola Road, south of Dale Circle, Morganton

This brick, Tudor Revival cottage has steeply pitched cross gables, a front chimney, an arcaded porch, both three-over-one and six-over-one windows, and decorative stone work. The house is in only fair condition and lacks any historical or architectural significance.

No. 11 House

East side of Enola Road, north of S.R. 1924, Morganton

This L-plan cottage has asbestos siding, two-over-two windows, a shed roofed porch with box piers, and a concrete block infill foundation. Now highly deteriorated, the house appears abandoned and no longer retains sufficient integrity to meet National Register eligibility.

No. 12 House

South side of unpaved road, just east of junction with Enola Road, Morganton

This hip roofed cottage retains its two-over-two windows, but has been asbestos sided, and the porch has been enclosed. Now heavily altered, the house no longer retains sufficient integrity to meet National Register eligibility.

No. 13 House

East side of Enola Road at junction with S.R. 1924, Morganton

This turn of the century, side gable dwelling has been extensively altered with four-over-four replacement windows and aluminum siding. The shed roofed porch is supported by later box piers resting on brick pedestals. There is a side addition, and the chimney has been rebuilt. The house lacks any architectural or historic significance and no longer retains sufficient integrity to meet National Register eligibility.

No. 14 House

East side of Enola Road, south of junction with S.R. 1924, Morganton

This hip roofed cottage has been completely altered with replacement siding and windows and numerous additions. Now heavily remodeled, the house no longer retains sufficient integrity to meet National Register eligibility.

No. 15 House

East side of Enola Road, south of junction with S.R. 1924, Morganton

This hip roofed cottage has a hip roofed dormer, engaged porch supported by battered piers on brick pedestals, one-over-one windows, and German siding. Although intact, the house lacks any special historical or architectural significance.

No. 16 House

East side of Enola Road, at intersection with Brick Street

This brick, Colonial Revival cottage has a side gable roof, front gable entry porch, and an arcaded side porch. The house has a symmetrical, three bay facade and eight-over-eight windows. Although intact, the house lacks any historical or architectural significance.

No. 17 Willis House

East side of Enola Road, north of Interstate 40, Morganton

The Willis House is a side gable bungalow with a front gable dormer, broad eaves, and a shed roofed porch supported by battered piers on brick piers. The house has been extensively altered with an added picture window, an added sliding glass door on a side elevation, and vinyl siding. The house has only marginal integrity and lacks any historical or architectural significance.

No. 18 House

East side of Enola Road, north of Interstate 40, Morganton

This front gable bungalow has a hip roofed porch supported by battered piers on brick piers. The house retains its three-over-one windows, but has been aluminum sided. The house lacks any historical or architectural significance.

No. 19 House

East side of Enola Road, south of Interstate 40, Morganton

This hip roofed cottage is now located between a gas station and a shopping center off Enola Road and may have been moved to the site. The house has a high hip roof, hip roofed dormers, two-over-two windows, vinyl siding, a concrete block foundation, and a wraparound porch with replacement piers. The house has been extensively altered and lacks any architectural integrity. Furthermore, the house may have been moved to the site, and in any event, no longer has its integrity of setting because of surrounding modern commercial construction.

No. 20 House

East side of Enola Road, south of Interstate 40, Morganton

This ca. 1900 hip roofed cottage has been highly altered with a brick veneer, a front addition, a replacement porch, and metal sash casement windows. The house has been extensively changed and lacks any architectural integrity.

No. 21 House

West side of Enola Road, north of junction with S.R. 1940, Morganton

This side gable bungalow has a front gable dormer, two-over-two windows, and a shed roofed porch supported by battered piers. The house retains some of its original German siding, but has been vinyl sided in places, and the chimney has been rebuilt. The property includes a concrete block well house and a collapsing frame shed. The house retains only marginal integrity and lacks architectural or historical significance.

No. 22 House

East side of Enola Road, north of junction with S.R. 1940, Morganton

This hip roofed cottage has been extensively altered with rebuilt chimneys, vinyl siding, replacement one-over-one windows, replacement doors, and a hip roofed porch with replacement box piers. The house has been extensively remodeled and lacks any architectural integrity.

No. 23 House

West side of Enola Road, north of junction with Brick Road

The frame, front gable bungalow has an engaged porch supported by battered piers on brick pedestals and three-over-one windows. The house lacks any historical or architectural significance.

Photographs

No. 4



No. 1



No. 5



No. 6



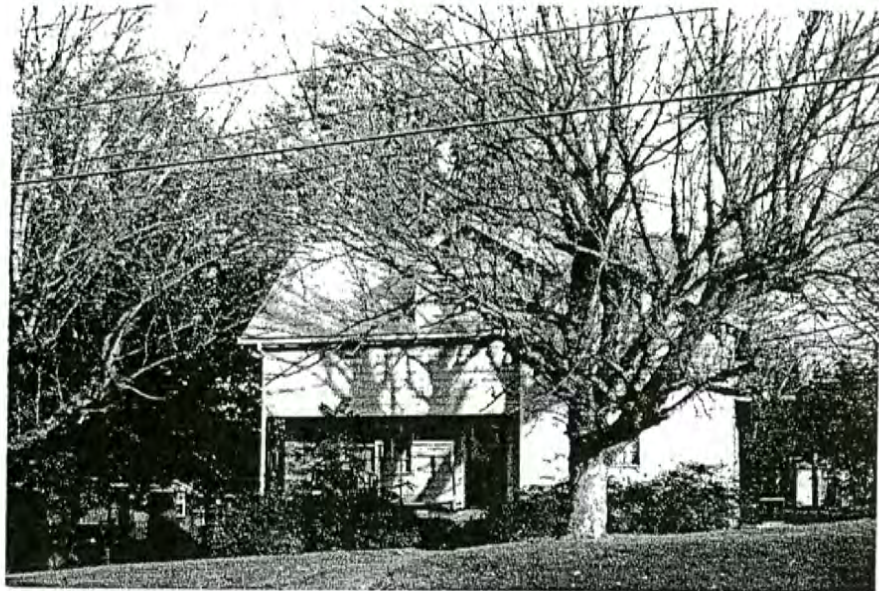
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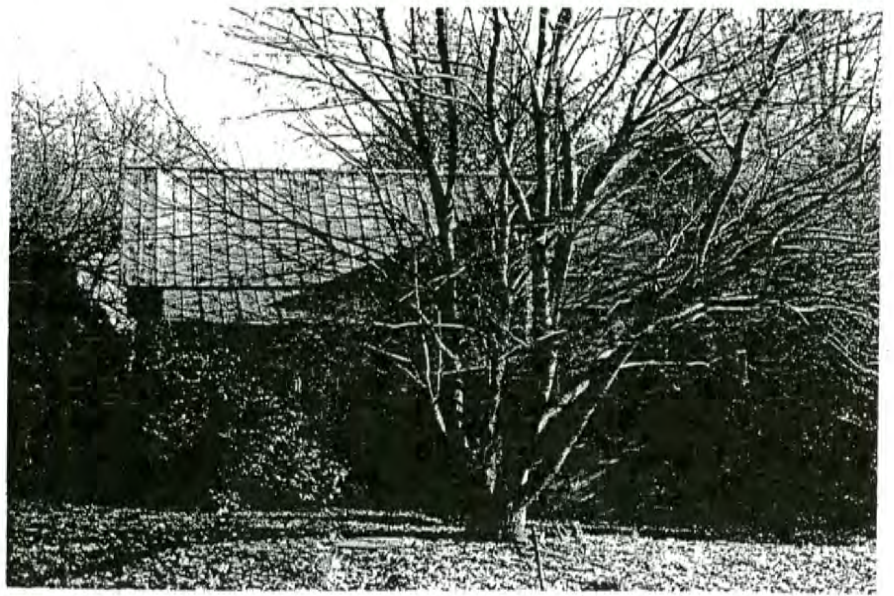
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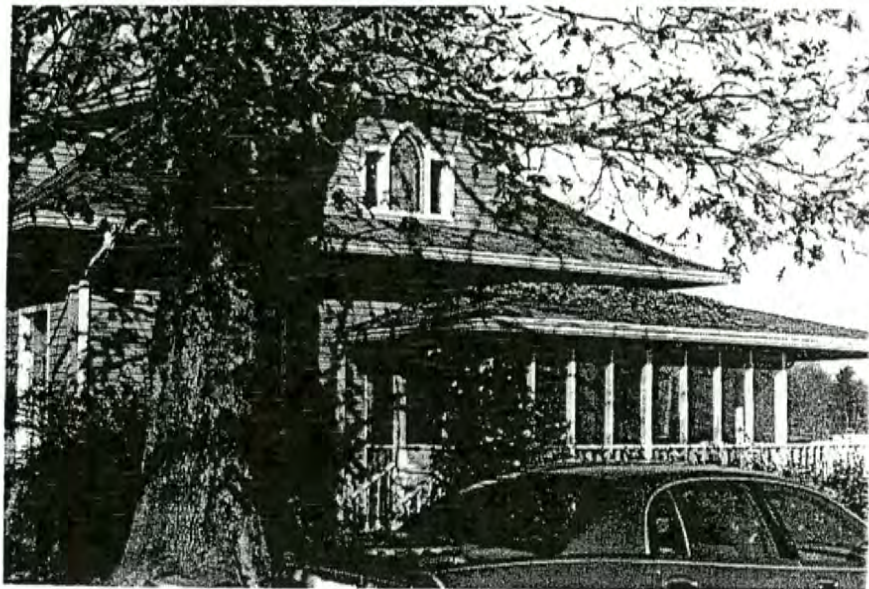
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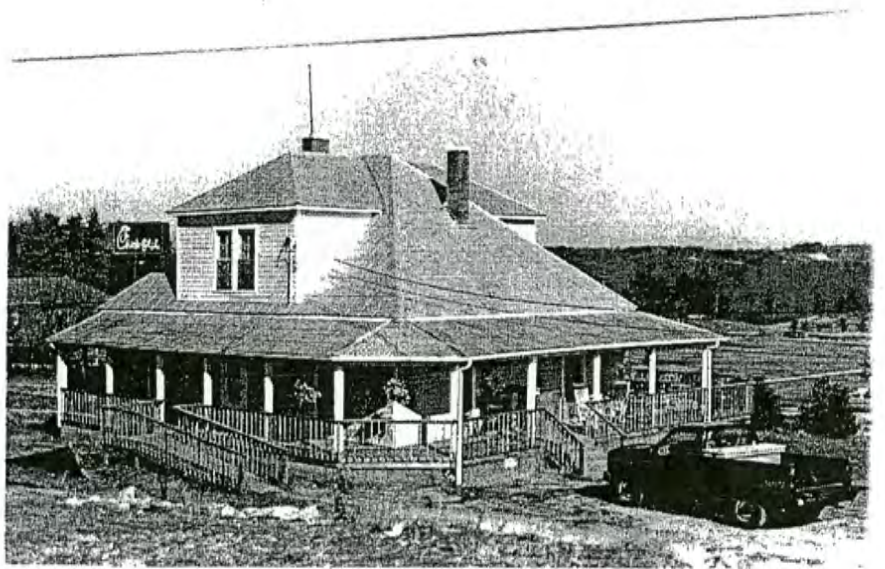
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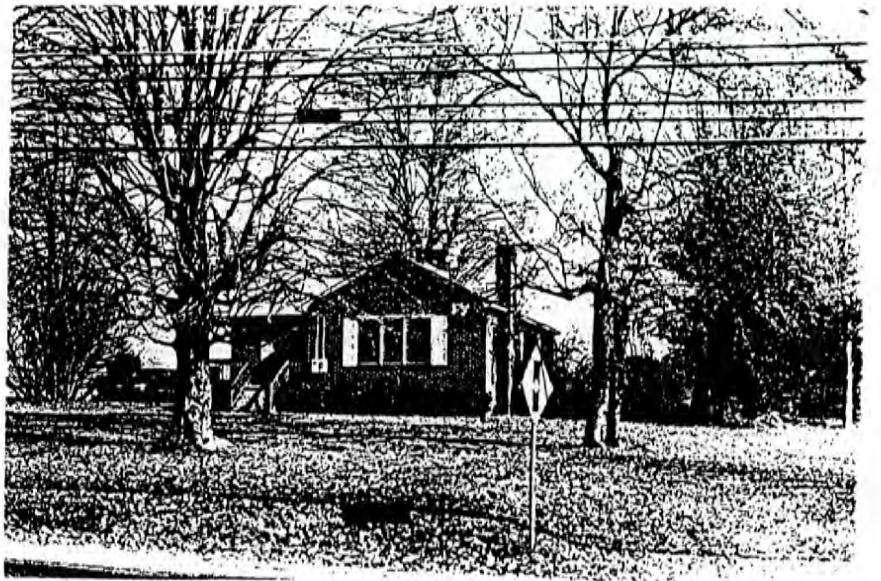
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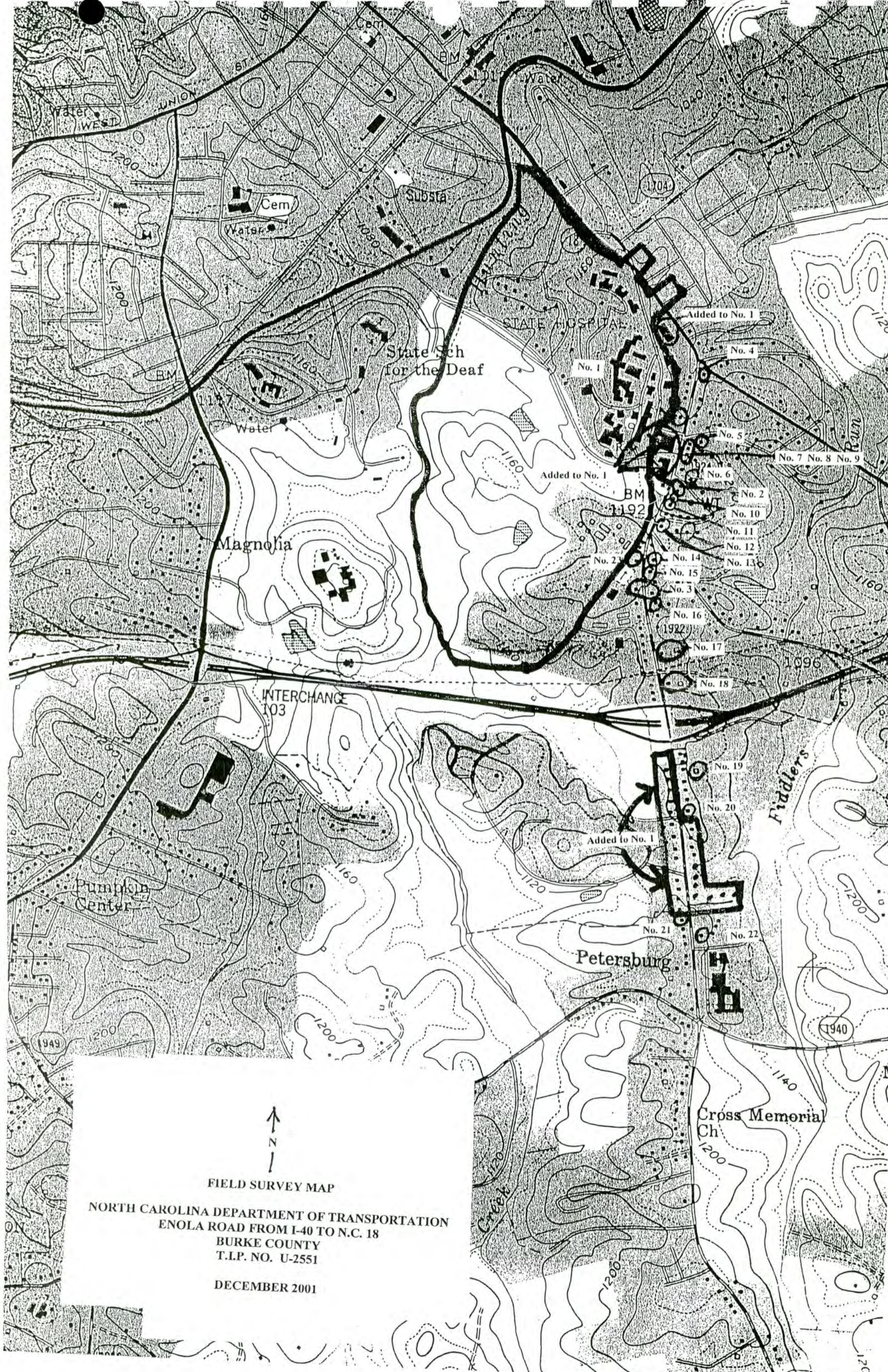


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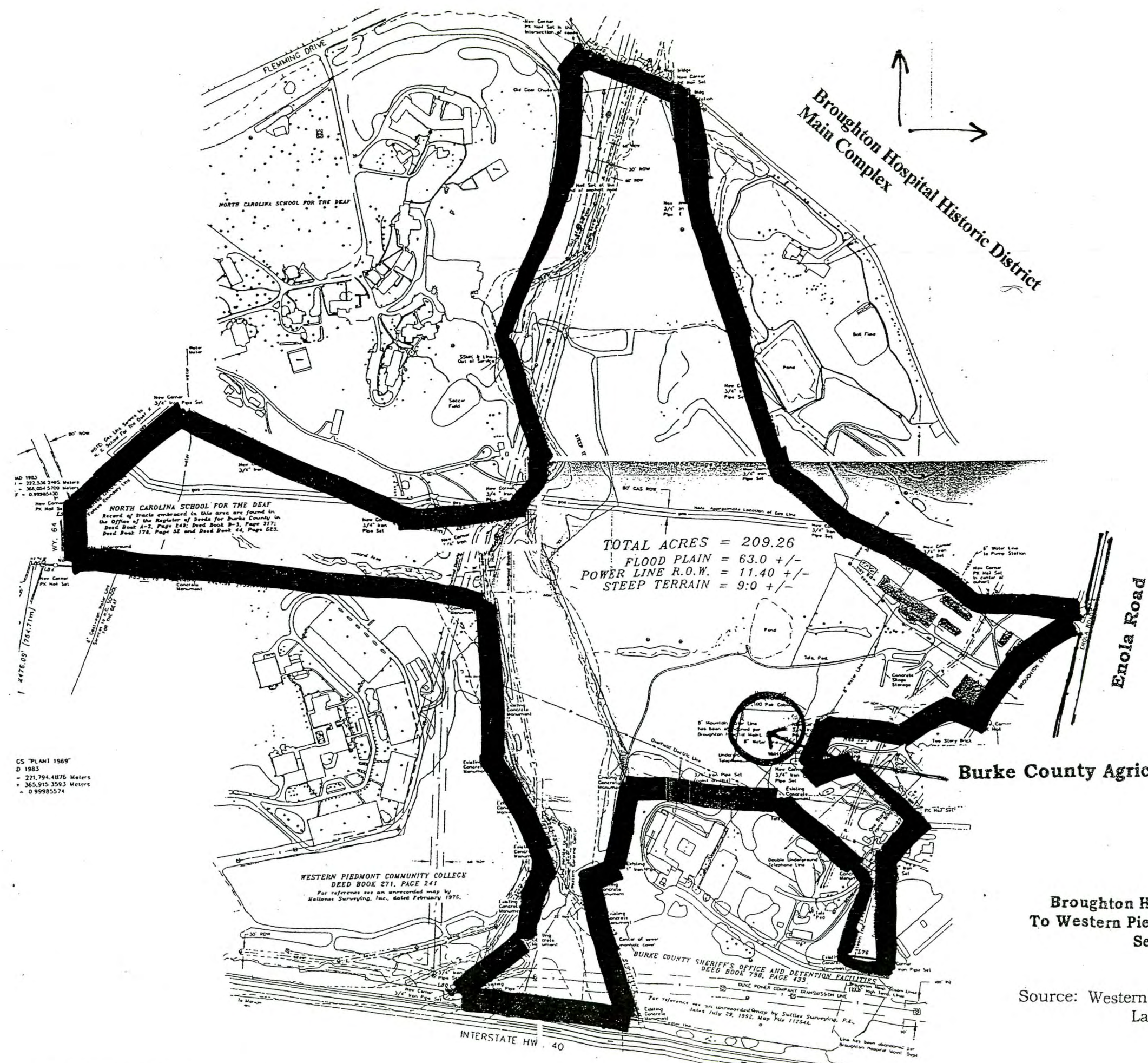
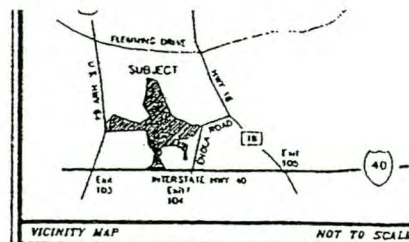




FIELD SURVEY MAP
NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
ENOLA ROAD FROM I-40 TO N.C. 18
BURKE COUNTY
T.I.P. NO. U-2551
DECEMBER 2001

APPENDIX B:

BROUGHTON LAND TRANSFER MAP



Enola Road

Burke County Agricultural Extension Center

Figure 15

Broughton Hospital Land Transferred
To Western Piedmont Community College
September 2000

Source: Western Piedmont Community College
Land Survey Map

APPENDIX C:

PROFESSIONAL QUALIFICATIONS

Frances P. Alexander
Architectural Historian

Education

- 1991 M.A. American Civilization-Architectural History
George Washington University
Washington, D.C.
- 1981 B.A. History with High Honors
Guilford College
Greensboro, North Carolina

Relevant Work Experience

- 1991-date Architectural Historian, Mattson, Alexander and Associates, Inc.
Charlotte, North Carolina
- 1988-1991 Department Head, Architectural History Department
Engineering-Science, Inc., Washington, D.C.
- 1987-1988 Architectural Historian, Historic American Buildings Survey/Historic
American Engineering Record, National Park Service, Washington, D.C.
- 1986-1987 Historian, National Register of Historic Places, National Park Service,
Washington, D.C.
- 1986 Historian, Historic American Engineering Record, National Park Service,
Chicago, Illinois

Richard L. Mattson, Ph.D.
Historical Geographer

Education

- 1988 Ph.D. Geography
University of Illinois, Urbana, Illinois
- 1980 M.A. Geography
University of Illinois, Urbana, Illinois
- 1976 B.A. History, Phi Beta Kappa
University of Illinois, Urbana, Illinois

Relevant Work Experience

- 1991-date Historical Geographer, Mattson, Alexander and Associates, Inc.
Charlotte, North Carolina
- 1991 Visiting Professor, History Department, Queens College, Charlotte, North Carolina
- Developed and taught course on the architectural history of the North Carolina Piedmont, focusing on African-American architecture, textile-mill housing, and other types of vernacular landscapes.
- 1989-1991 Mattson and Associates, Historic Preservation Consulting
Charlotte, North Carolina
- 1988 Visiting Professor, Department of Urban and Regional Planning,
University of Illinois, Urbana, Illinois
- Taught historic preservation planning workshop, developed and taught course on the history of African-American neighborhoods. The latter course was cross-listed in African-American Studies.
- 1984-1989 Private Historic Preservation Consultant,
Raleigh, North Carolina
- 1981-1984 Academic Advisor, College of Liberal Arts and Sciences, University of
Illinois, Urbana, Illinois
- 1981 Instructor, Department of Geography, University of Illinois, Urbana,
Illinois
- 1978-1980 Private Historic Preservation Consultant, Champaign, Illinois